

POSITIVES:

- (+) PERMITS BUS ROUTE TO ACCESS LAND USES MORE DIRECTLY.
- (+) POTENTIAL FOR SHARED USE OF OVERHANG FOR BUS PATRONS DURING INCLEMENT WEATHER.
- (+) REDUCES WALKING TIME AND DISTANCE FROM THE LAND USE TO THE BUS STOP.
- (+) REDUCES THE POTENTIAL FOR VEHICULAR/PEDESTRIAN CONFLICTS IN THE PARKING LOT.
- (+) PATRON SECURITY MAY BE ENHANCED THROUGH PROXIMITY TO LAND USE. INDIRECT SURVEILLANCE FROM THE LAND USE MAY BE INCREASED AND THE NUMBER OF POTENTIAL HIDING PLACES IS REMOVED BY PLACING THE STOP ADJACENT TO THE BUILDING.

NEGATIVES:

- (-) BUS/GENERAL VEHICLE CONFLICTS MAY INCREASE BY HAVING THE ROUTE DEVIATE INTO THE PARKING AREA.
- (-) ROUTE TRAVEL TIME AND DISTANCE ARE INCREASED.



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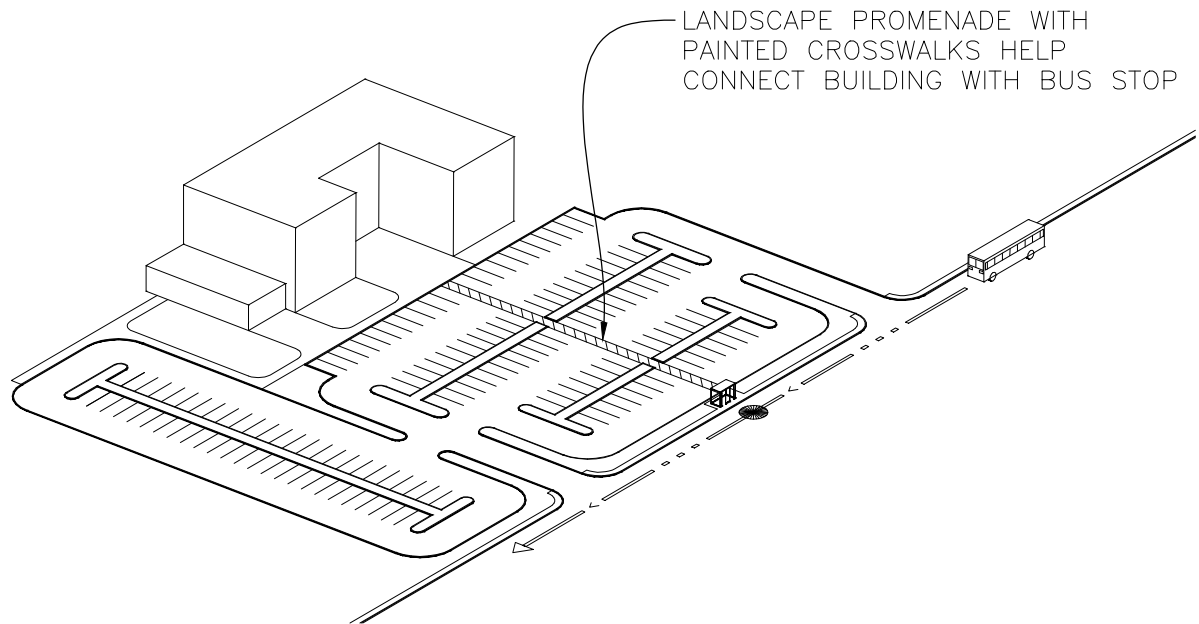
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**FIGURE No. 2-1  
HYPOTHETICAL MEDICAL CENTER**



POSITIVES:

(+) BUS REMAINS ON A MAIN THOROUGHFARE, MINIMIZING TRIP TIME AND DISTANCE.

NEGATIVES:

(-) DOES NOT REDUCE WALKING DISTANCE OR TIME BETWEEN THE LAND USE AND THE BUS STOP.

(-) PATRON SECURITY MAY STILL BE COMPROMISED IF THE PROMENADE IS NOT WELL USED, WELL-LIT, OR SIGHT-LINES ARE RESTRICTED BY VEGETATION.

(-) PEDESTRIAN/VEHICULAR CONFLICTS IN PARKING LOT ARE INCREASED.



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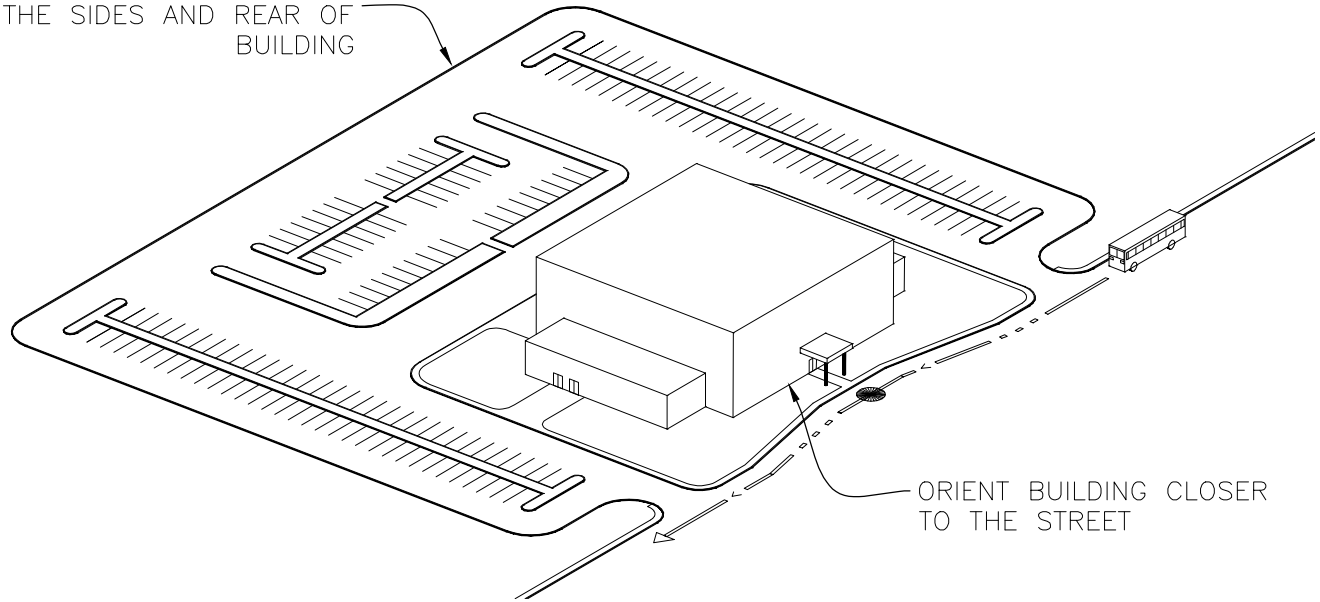
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**FIGURE No. 2-2**  
**HYPOTHETICAL**  
**OFFICE BUILDING COMPLEX**

KEEP PARKING CELLS TO THE SIDES AND REAR OF BUILDING



ORIENT BUILDING CLOSER TO THE STREET

POSITIVES:

- (+) TRANSIT PASSENGER WALKING TIME AND DISTANCE IS REDUCED SINCE THE BUILDING IS NEAR THE ROAD.
- (+) PATRON SECURITY IS ENHANCED BY HAVING INDIRECT SURVEILLANCE FROM THE BUILDING AND PASSING VEHICULAR TRAFFIC.
- (+) POTENTIAL FOR PEDESTIAN/VEHICULAR CONFLICTS ARE REDUCED BETWEEN THE LAND USE AND THE BUS STOP.
- (+) POTENTIAL FOR SHARED USE OF THE BUILDING FACILITIES, SUCH AS OVERHANGS AND ATRIUMS, BY BUS PATRON DURING INCLEMENT WEATHER.
- (+) BUS REMAINS ON MAIN ROUTE BY ELIMINATING THE NEED TO DEVIATE INTO A PARKING LOT.

NEGATIVES:

- (-) TRANSIT FACILITIES CHALLENGES TRADITIONAL LAND USE PRACTICES, WHICH MAY MAKE COMMUNITIES MORE RELUCTANT TO IMPLEMENT SUCH A STRATEGY.
- (-) CONFUSION MAY DEVELOP CONCERNING RESPONSIBILITIES FOR THE MAINTENANCE AND UP-KEEP OF A BUS STOP THAT IS NEAR A MAJOR GENERATOR OF ACTIVITY.
- (-) BUS TURNOUT MAY BECOME MAJOR DROP OFF ENTRANCE
- (-) BUS STOP AT ENTRANCE MAY NOT BE ARCHITECTURALLY DESIRABLE.



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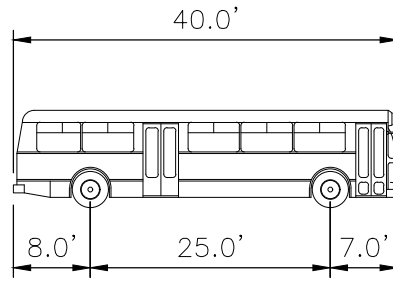
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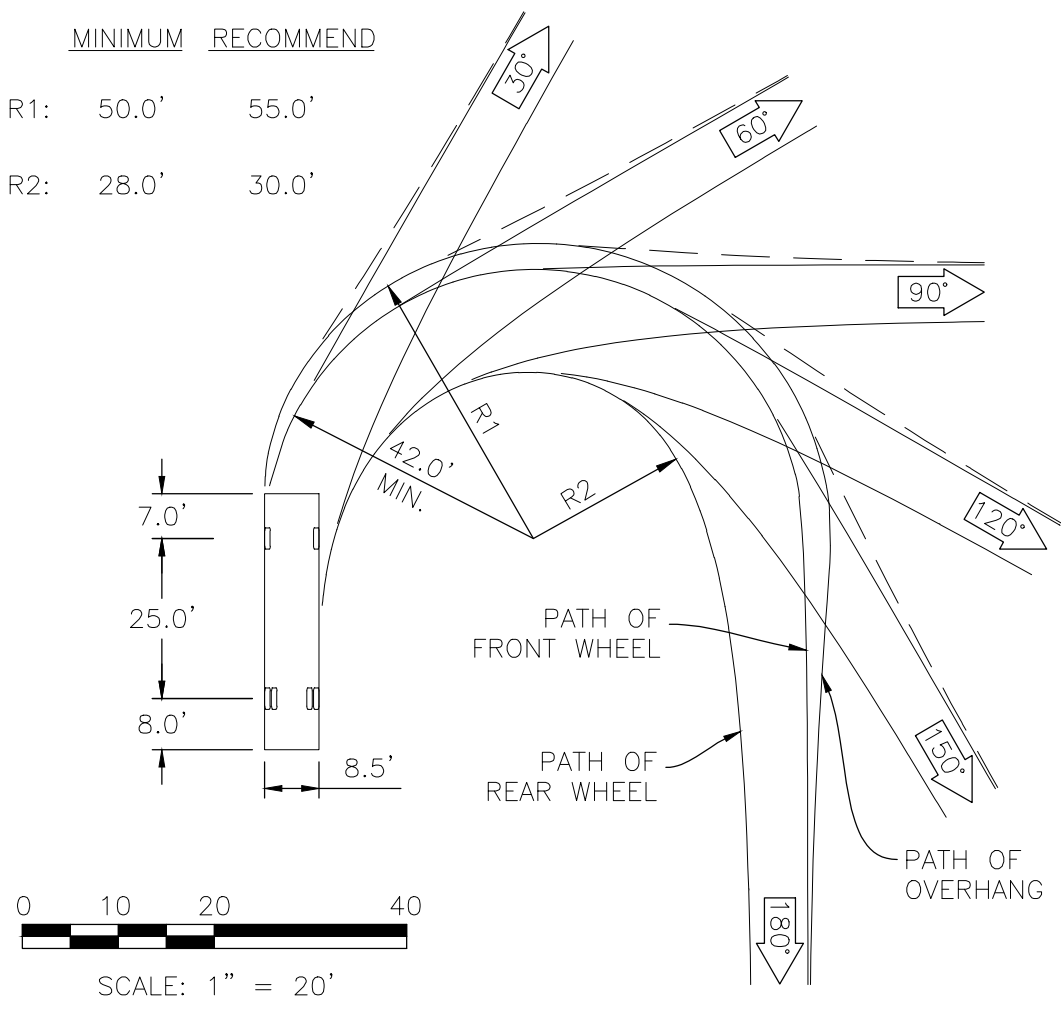
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**FIGURE No. 2-3  
HYPOTHETICAL RETAIL CENTER**

BUS ITEM	DISTANCE
WIDTH	8.5'
TRACK	8.5'



	MINIMUM	RECOMMEND
R1:	50.0'	55.0'
R2:	28.0'	30.0'



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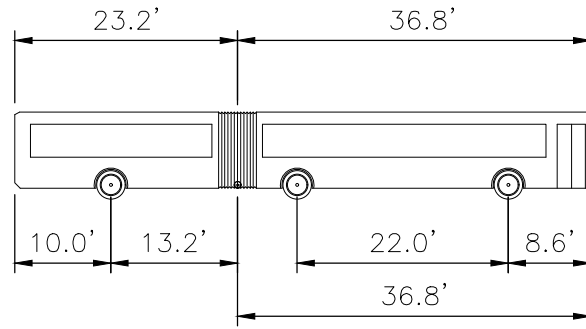
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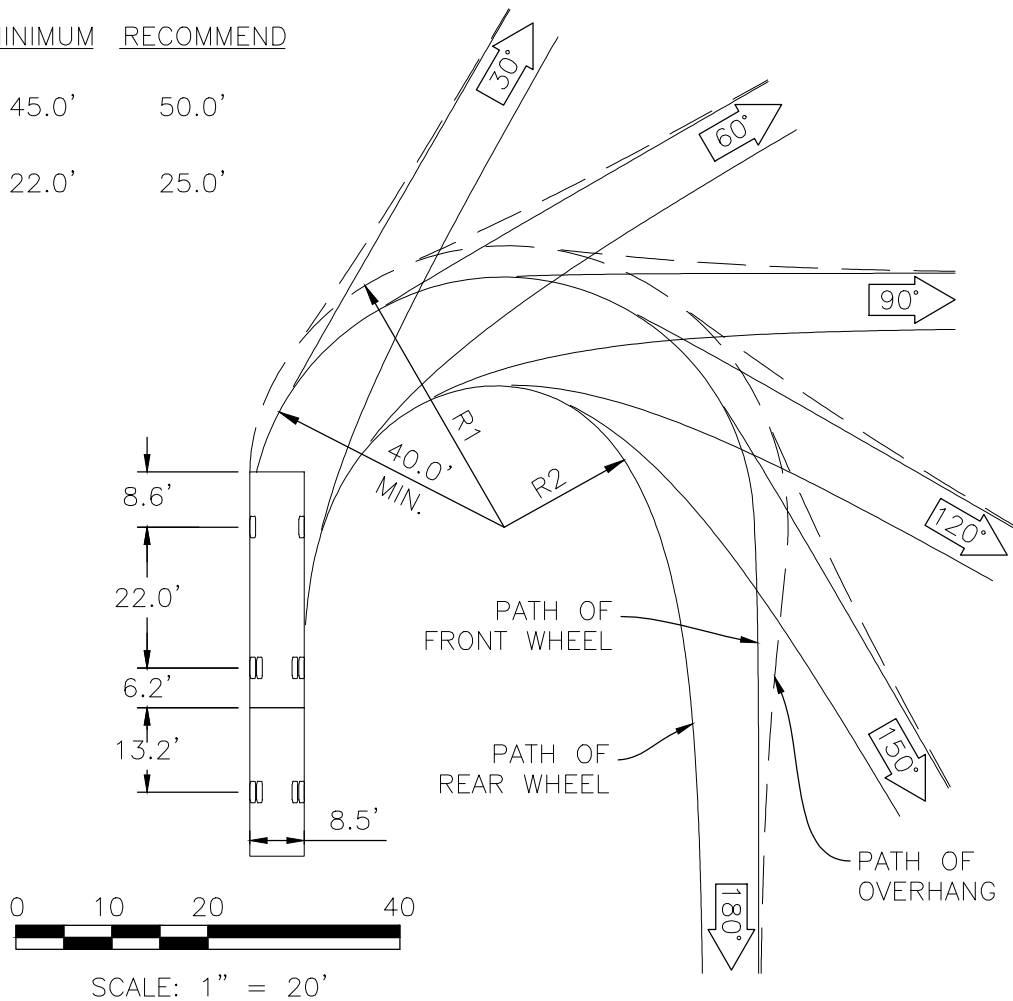
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**FIGURE No. 3-1**  
**40' BUS TURNING RADII**

BUS ITEM	DISTANCE
WIDTH	8.5'
TRACK	8.5'



	MINIMUM	RECOMMEND
R1:	45.0'	50.0'
R2:	22.0'	25.0'



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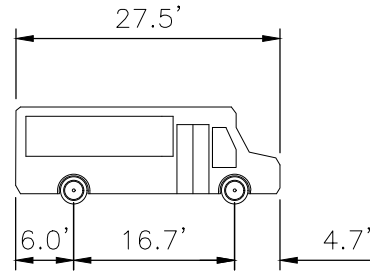
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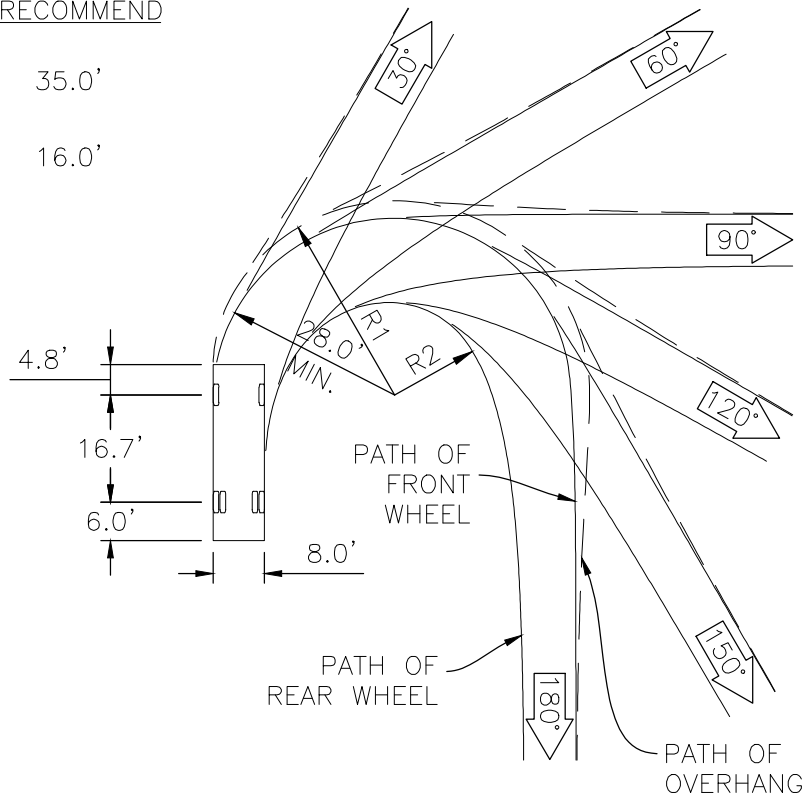
DATE:

FIGURE No. 3-2  
ARTICULATED BUS TURNING RADII

BUS ITEM	DISTANCE
WIDTH	8.0'
TRACK	8.0'



	MINIMUM	RECOMMEND
R1:	31.0'	35.0'
R2:	14.0'	16.0'



SCALE: 1" = 20'



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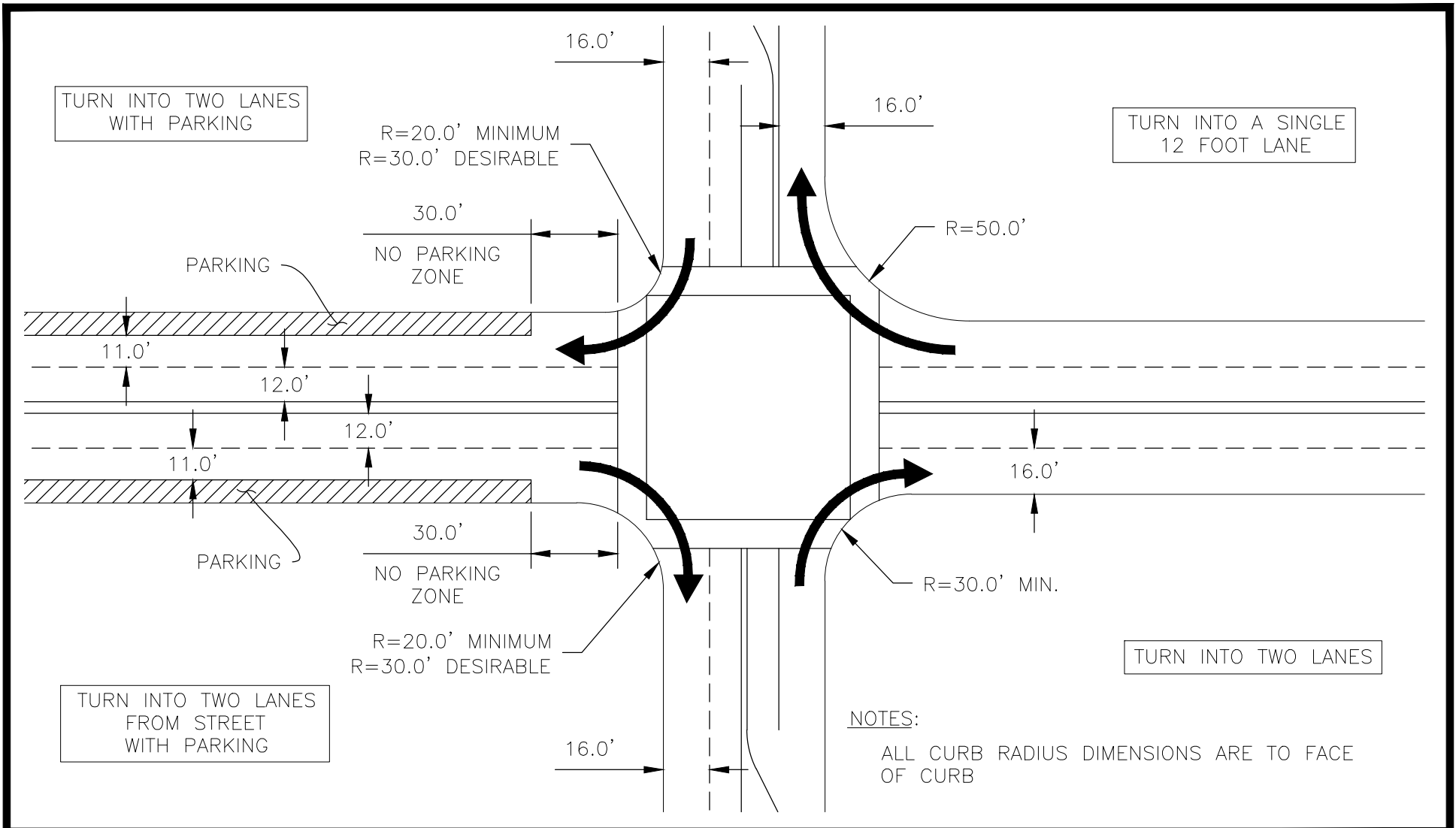
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FIGURE No. 3-3  
COMMUTER BUS TURNING RADII



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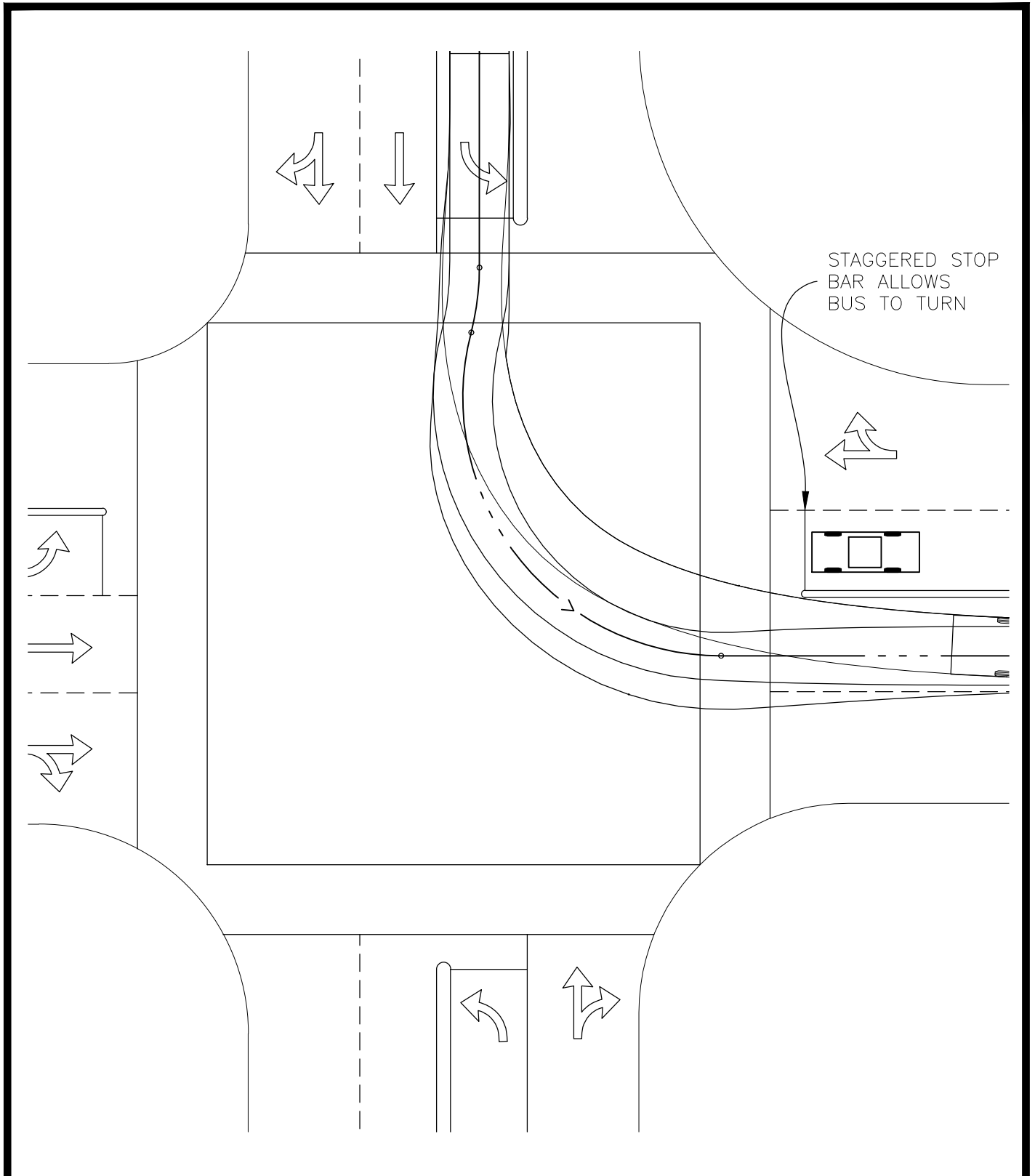
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**FIGURE No. 3-4**

**CURB DESIGN FOR BUS TURNING**



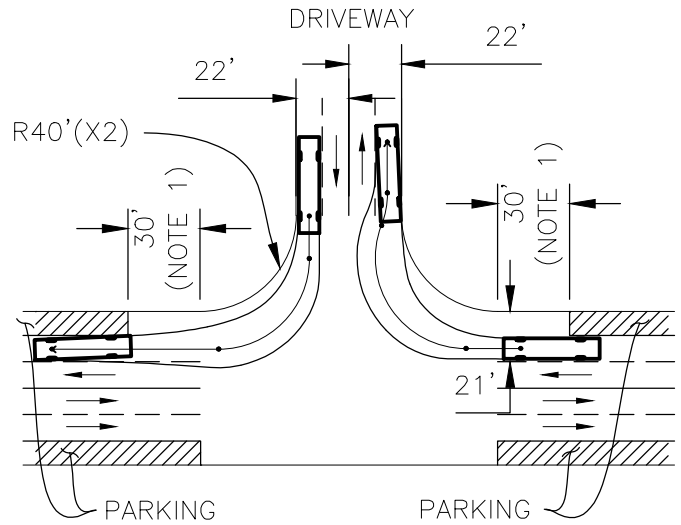
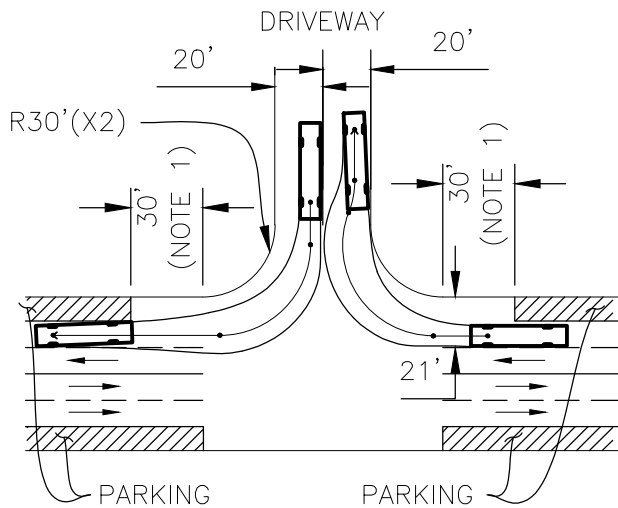
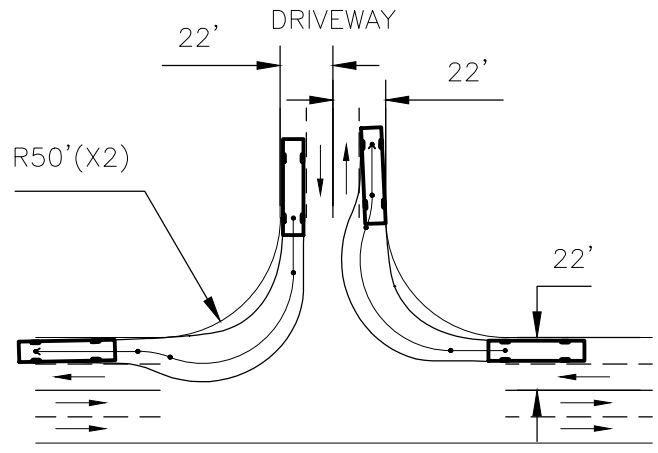
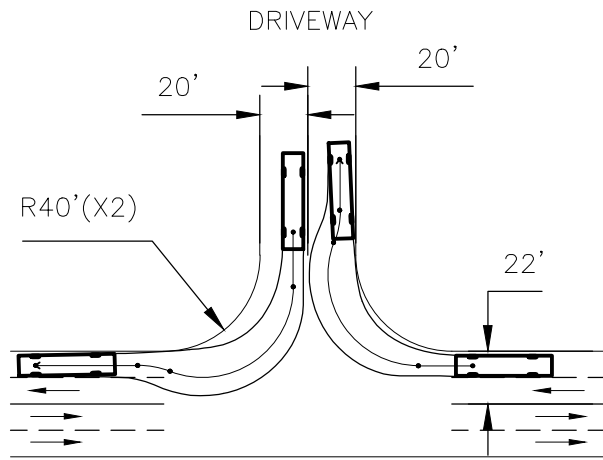

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**FIGURE No. 3-5**  
**STOP LINE RELOCATION**





NOTE:

1. PARKING SHOULD BE PROHIBITED FOR 30' WHERE BUSES MAKE A RIGHT-TURN AND HEAVY VEHICLE MOVEMENT OCCURS OR IS ANTICIPATED.



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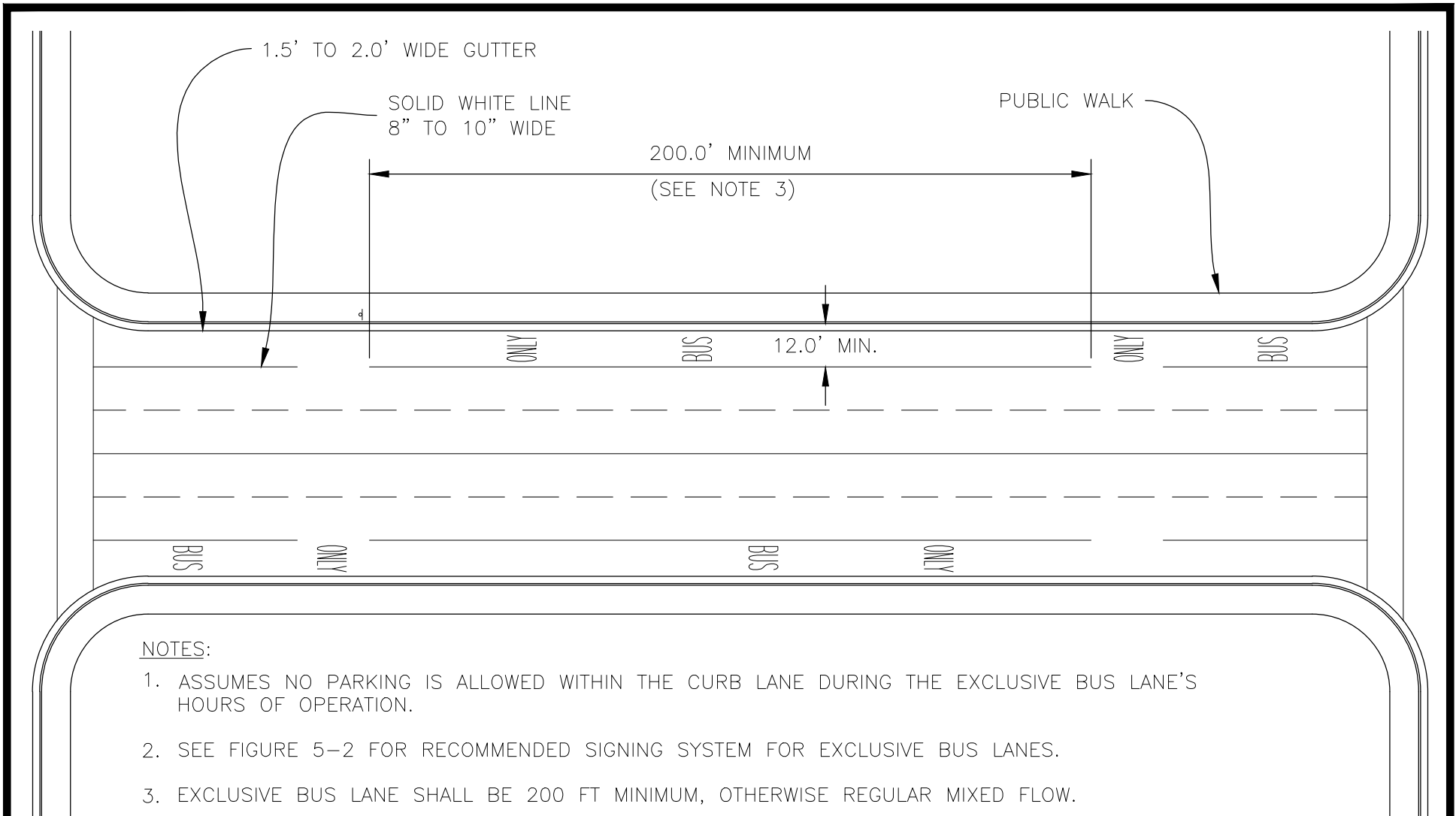
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**FIGURE NO. 3-6  
BUS DRIVEWAY RADIUS**



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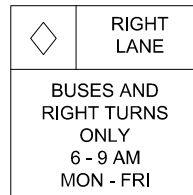
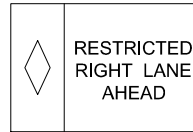
**FIGURE No. 5-1  
EXCLUSIVE BUS LANE**

INFORMATION REQUIREMENTS

SIGNING SYSTEM

APPROACH

- EXISTENCE
- LOCATION
  
- LOCATION
- VEHICLE RESTRICTION
- TIME OF OPERATION
- RULES OF USE

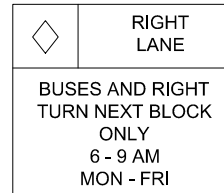


MUTCD – "NO PARKING 7-9 AM" SIGNS

ENTRANCE

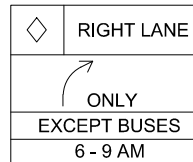
- VEHICLE RESTRICTION
- TIME OF OPERATION
- LOCATION
- RULES OF USE

APPROACH SIGN OR:



THROUGH

- VEHICLE RESTRICTION
- TIME OF OPERATION
- LOCATION
- RULES OF USE



AT EVERY INTERSECTION WHERE RIGHT TURN IS PERMITTED

EXIT

- END OF FACILITY
- HOW TO EXIT



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**DESIGN GUIDELINES**

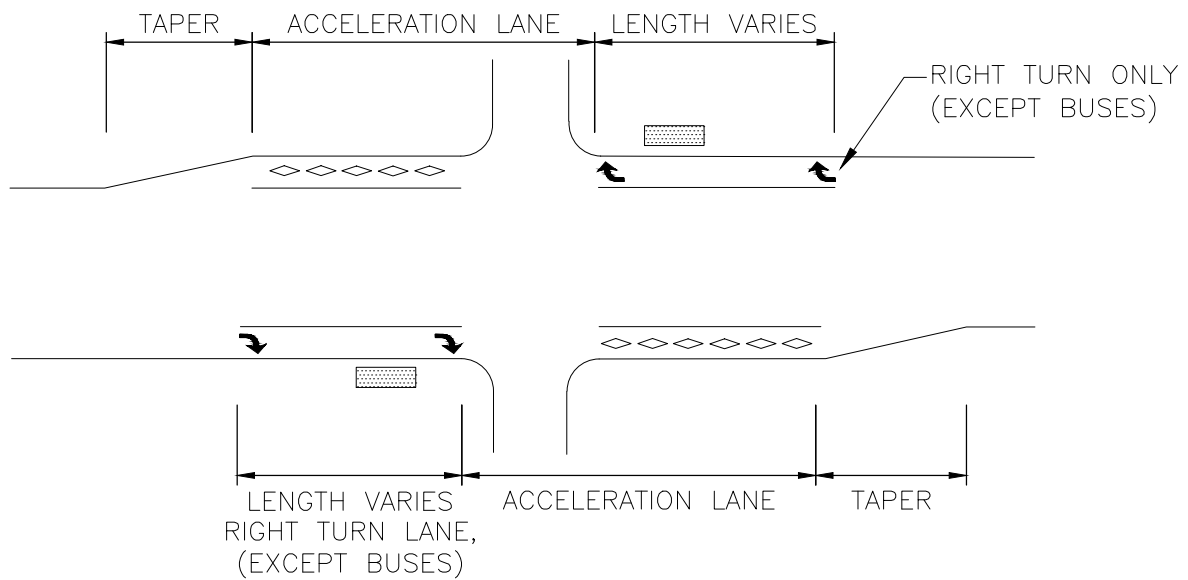
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**FIGURE No. 5-2  
RECOMMENDED SIGNING SYSTEM  
FOR EXCLUSIVE LANES**

## QUEUE JUMPER WITH NEARSIDE BUS STOP AND ACCELERATION LANE



**LEGEND:**

-  BUS STOP
-  EXCLUSIVE BUS ONLY LANE



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## DESIGN GUIDELINES

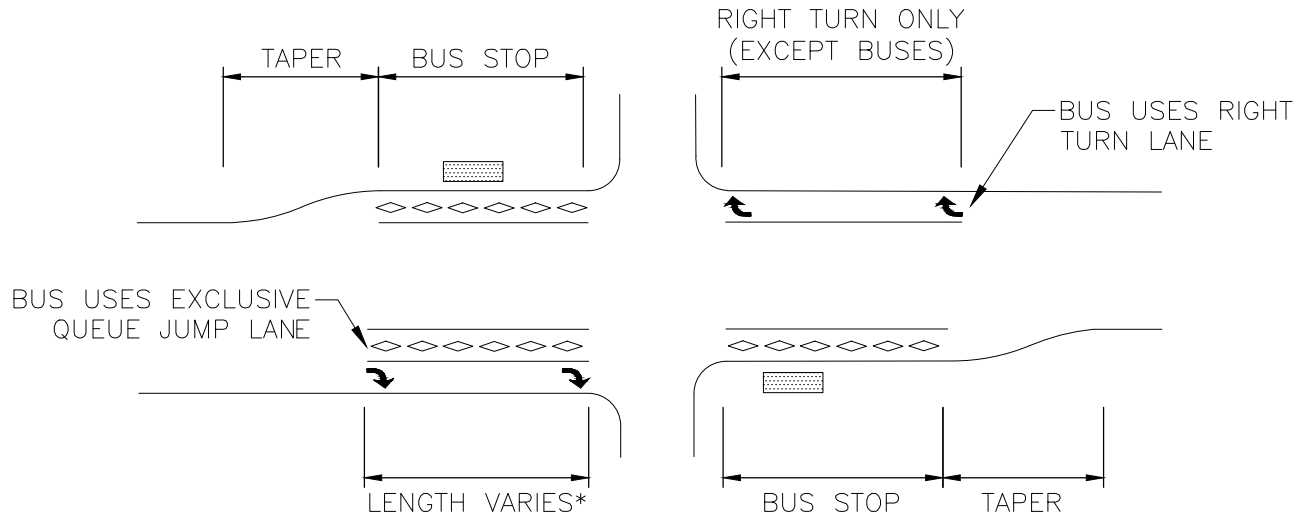
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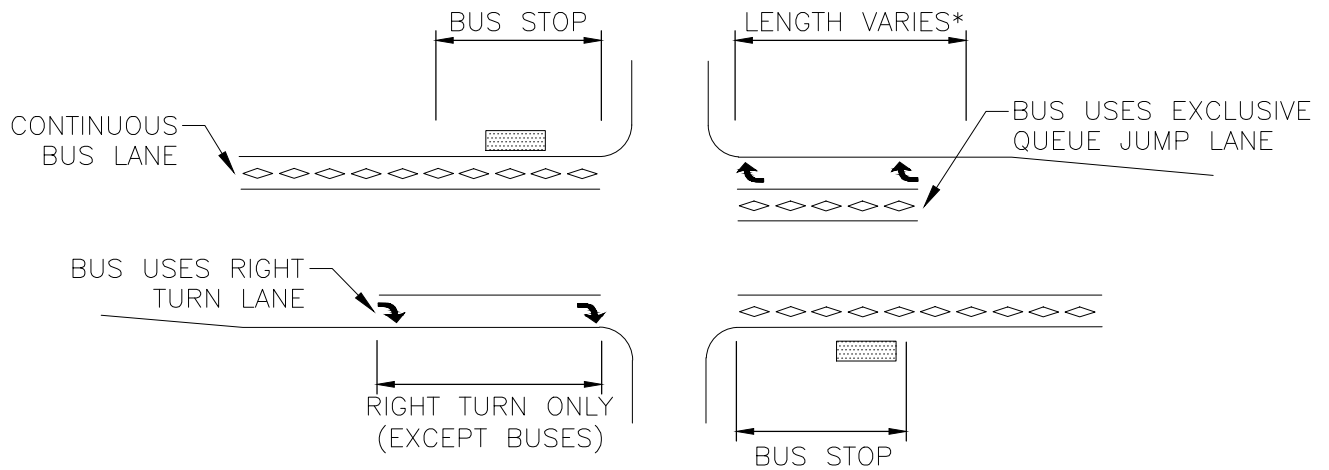
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**FIGURE No. 6-1  
QUEUE JUMPER OPTIONS  
NEARSIDE BUS STOP**

## QUEUE JUMPER WITH FARSIDE BUS STOP



## QUEUE JUMPER WITH CONTINUOUS BUS LANE AND FARSIDE BUS STOP



**LEGEND:**

BUS STOP

EXCLUSIVE BUS ONLY LANE

\* EXTEND QUEUE JUMP LANE AS NECESSARY TO BYPASS TRAFFIC QUEUE



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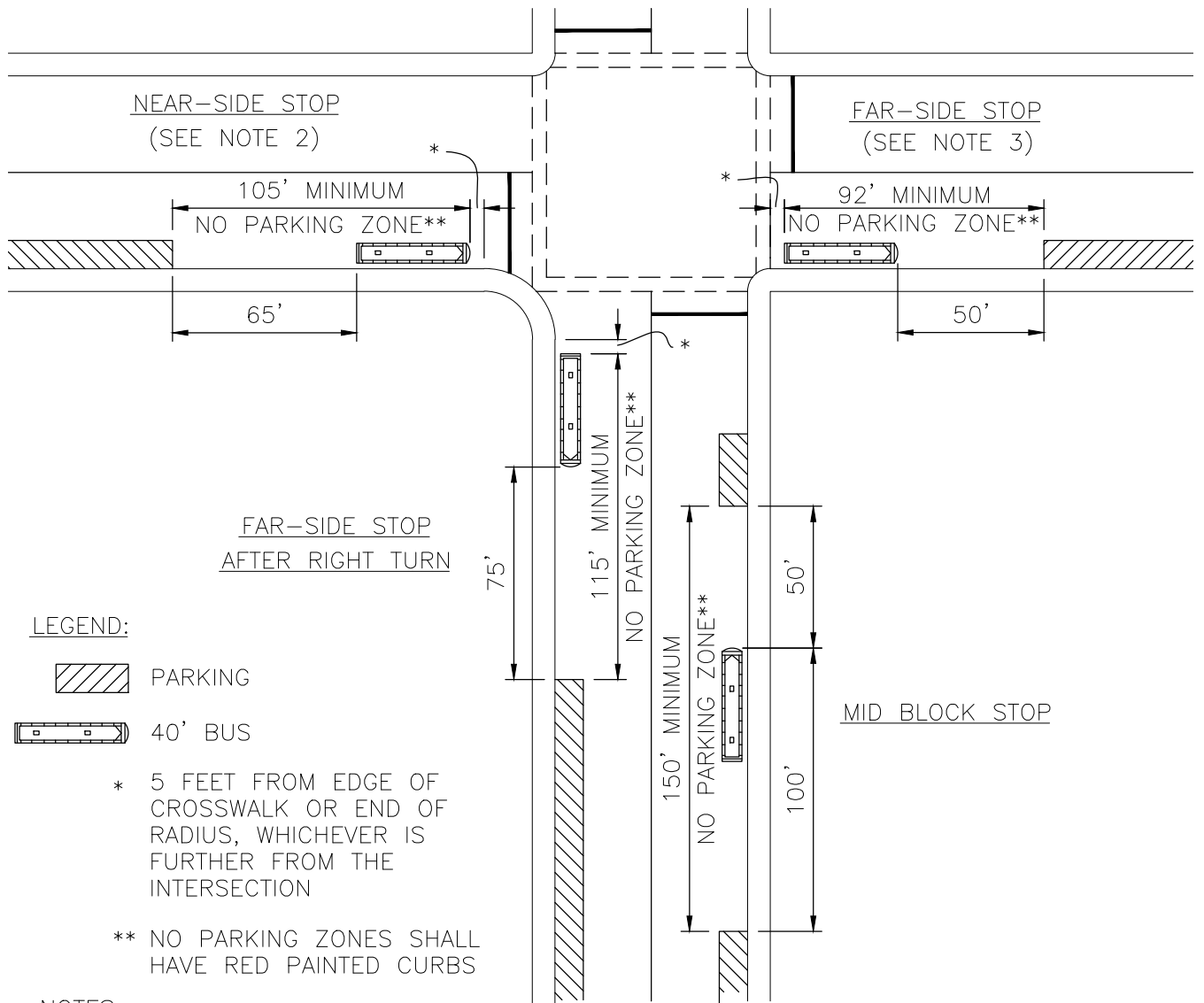
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**FIGURE No. 6-2  
QUEUE JUMPER OPTIONS  
FARSIDE BUS STOP**



NOTES:

1. DIMENSIONS ASSUME PARKING PROHIBITED ALONG ENTIRE LENGTH OF BUS STOP.
2. FOR NEAR SIDE STOPS INCREASE DIMENSIONS 15' WHERE BUSES MAKE A RIGHT TURN. IF ANY RIGHT TURN VEHICLE MOVEMENT OCCURS, OR IS ANTICIPATED, INCREASE MINIMUM DIMENSION 30'.
3. DIMENSIONS ARE FOR NO RIGHT TURN ON RED. INCREASE 90' DIMENSION BY 25' IF A RIGHT TURN CAN BE MADE BY BUSES.
4. INCREASE BUS STOP ZONE BY 50' FOR EACH ADDITIONAL STANDARD 40' BUS OR 70' FOR EACH ADDITIONAL 60' ARTICULATED BUS EXPECTED TO BE AT THE STOP SIMULTANEOUSLY. SEE TABLE 3.2 FOR THE SUGGESTED BUS STOP CAPACITY REQUIREMENTS BASED ON RANGE OF BUS FLOW RATES AND PASSENGER SERVICE TIMES.



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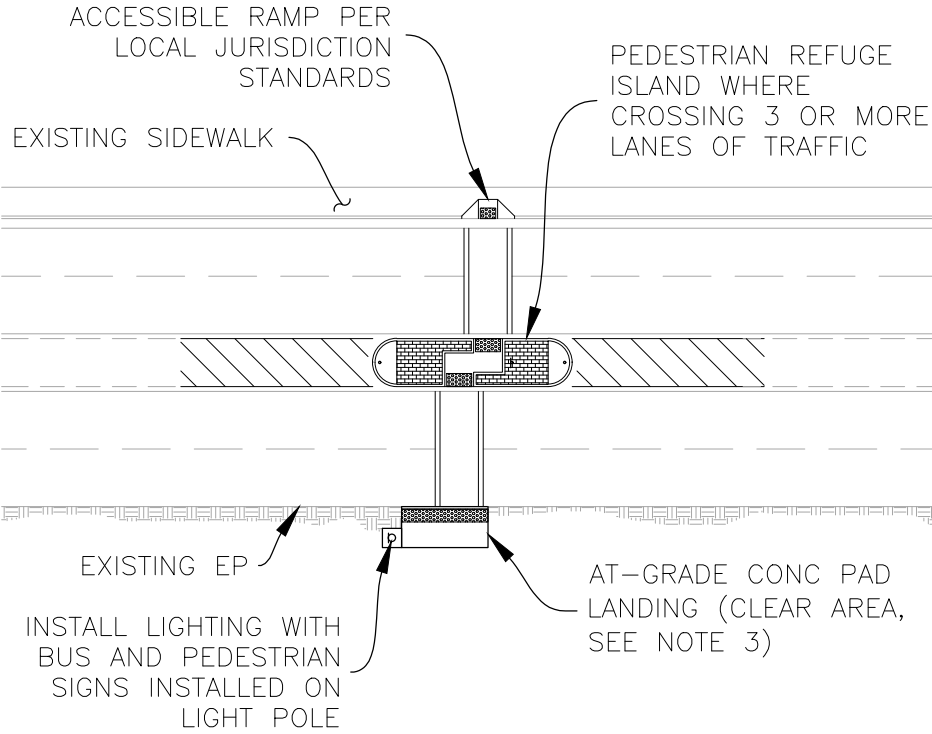
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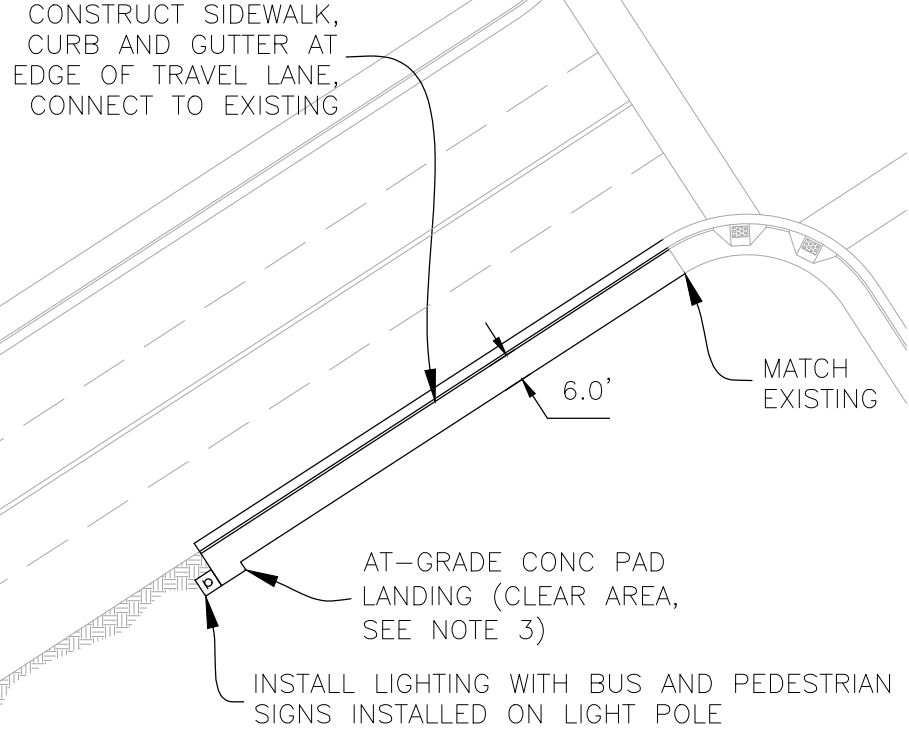
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**FIGURE No. 7-1  
TYPICAL DIMENSIONS  
FOR ON-STREET BUS STOPS**



MID-BLOCK CROSSING OF EXISTING ROADWAY



CONNECTING TO EXISTING INTERSECTION

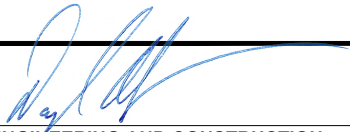
NOTES:

1. WHERE HIGH PEDESTRIAN AND PASSENGER VOLUMES OCCUR, SIDEWALK MUST BE 8 FEET.
2. ALL SIGNAGE, STRIPING AND PAVEMENT MARKINGS MUST BE IN ACCORDANCE WITH CALIFORNIA MUTCD.
3. REFER TO FIGURE NO. 7-6 FOR FOR BUS STOP PAD MINIMUM DIMENSIONS.



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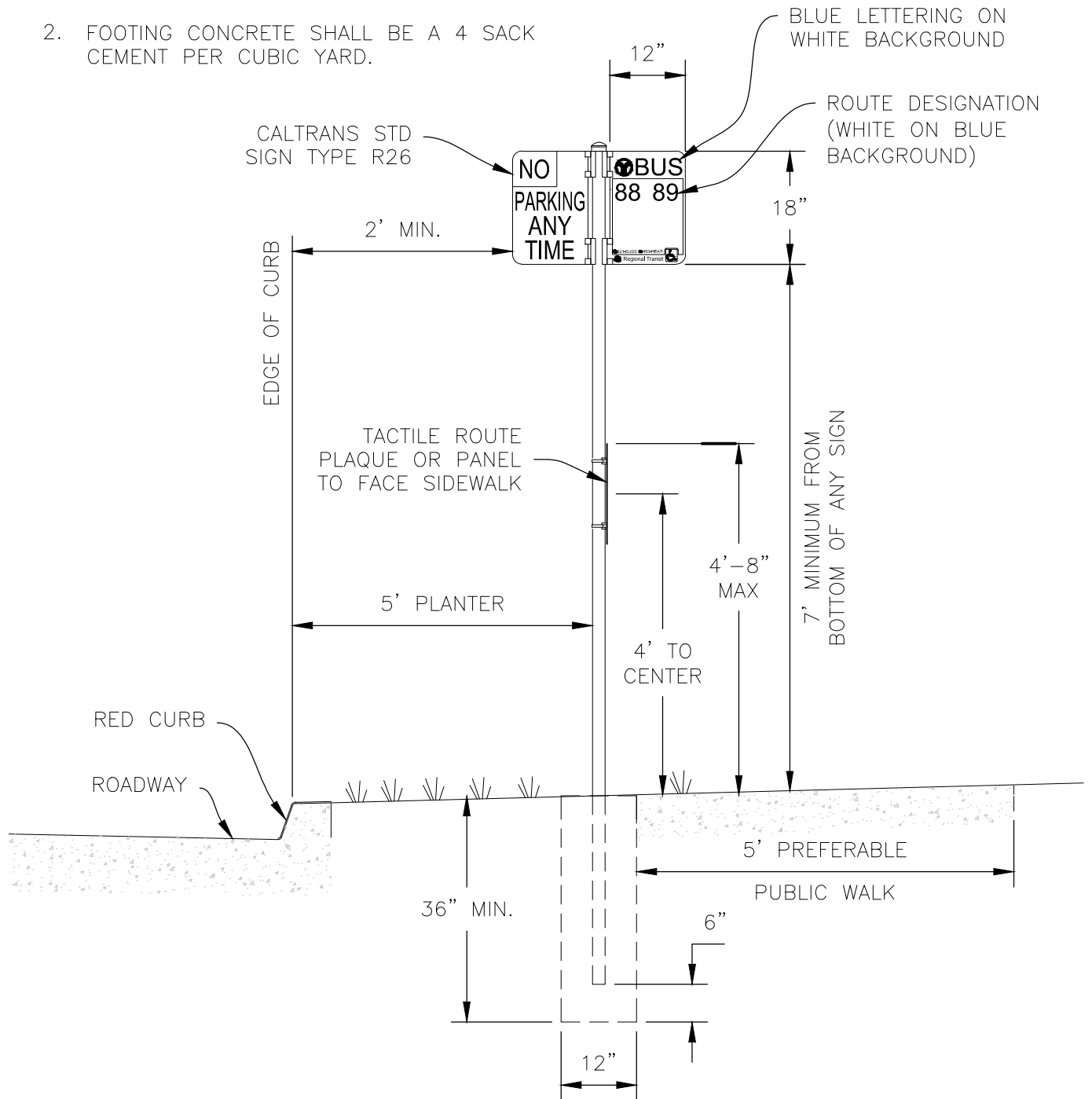
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**FIGURE No. 7-2**  
**TEMPORARY BUS STOP**  
**BUS STOPS AT UNIMPROVED LOCATIONS**

NOTES:

1. POLE SHALL BE 2 $\frac{3}{8}$ " OUTSIDE DIAMETER GALVANIZED STEEL PIPE WITH A MINIMUM WALL THICKNESS OF 0.116".
2. FOOTING CONCRETE SHALL BE A 4 SACK CEMENT PER CUBIC YARD.



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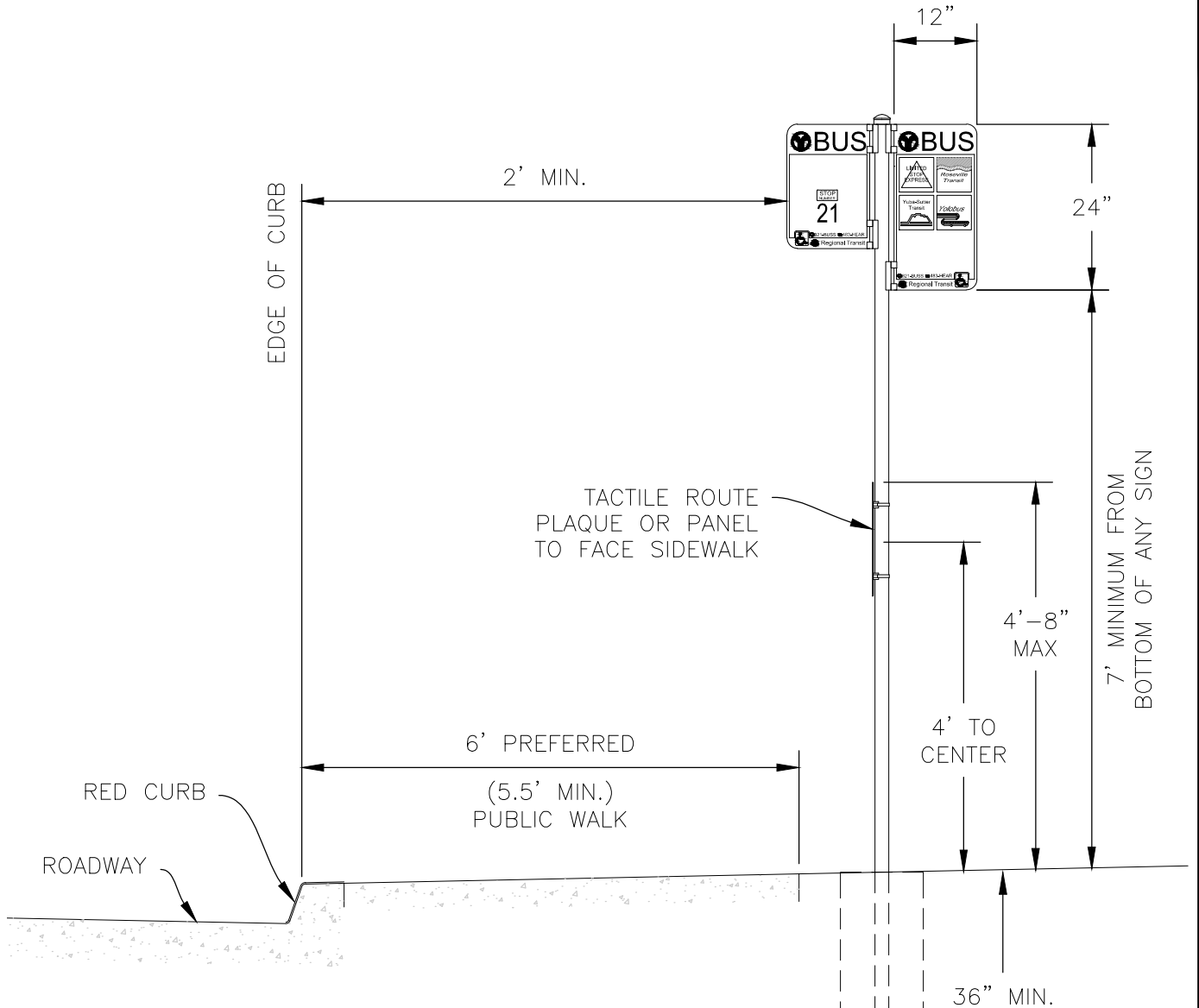
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DATE: \_\_\_\_\_

**FIGURE No.7-3 (a)  
BUS STOP SIGNAGE  
(SEPARATED SIDEWALK)**





NOTES:

1. POLE SHALL BE 2 $\frac{3}{8}$ " OUTSIDE DIAMETER GALVANIZED STEEL PIPE WITH A MINIMUM WALL THICKNESS OF 0.116".
2. FOOTING CONCRETE SHALL BE A 4 SACK CEMENT PER CUBIC YARD.

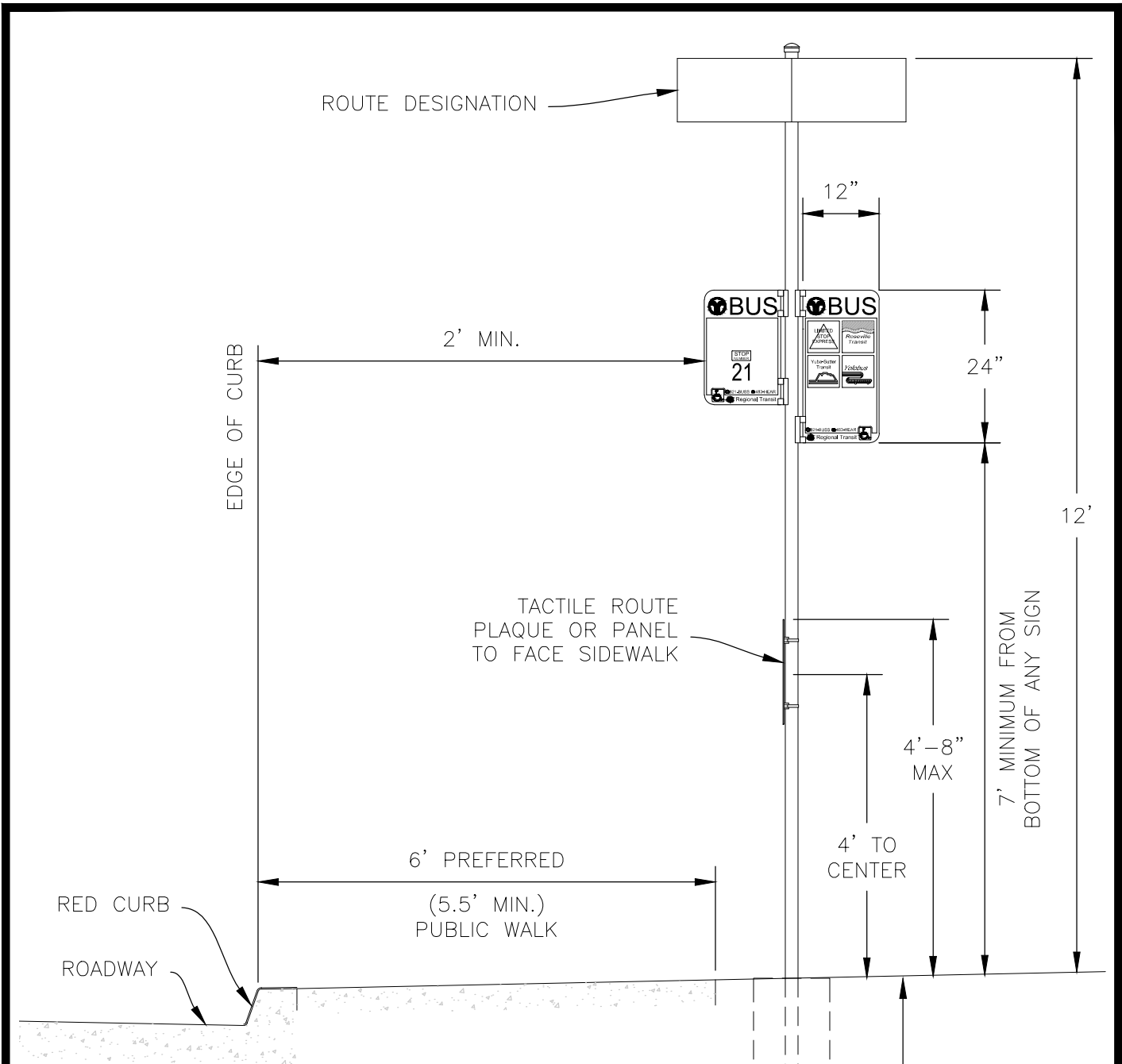


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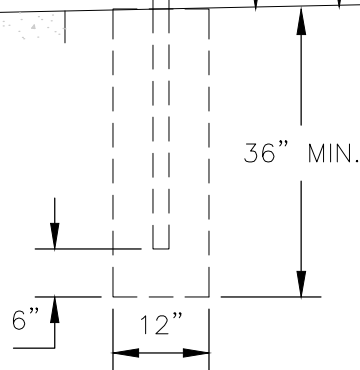
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**FIGURE No.7-3 (b)  
 BUS STOP SIGNAGE  
 (SIDEWALK ADJACENT TO CURB)**



**NOTES:**

1. POLE SHALL BE 2<sup>3</sup>/<sub>8</sub>" OUTSIDE DIAMETER GALVANIZED STEEL PIPE WITH A MINIMUM WALL THICKNESS OF 0.116".
2. FOOTING CONCRETE SHALL BE A 4 SACK CEMENT PER CUBIC YARD.



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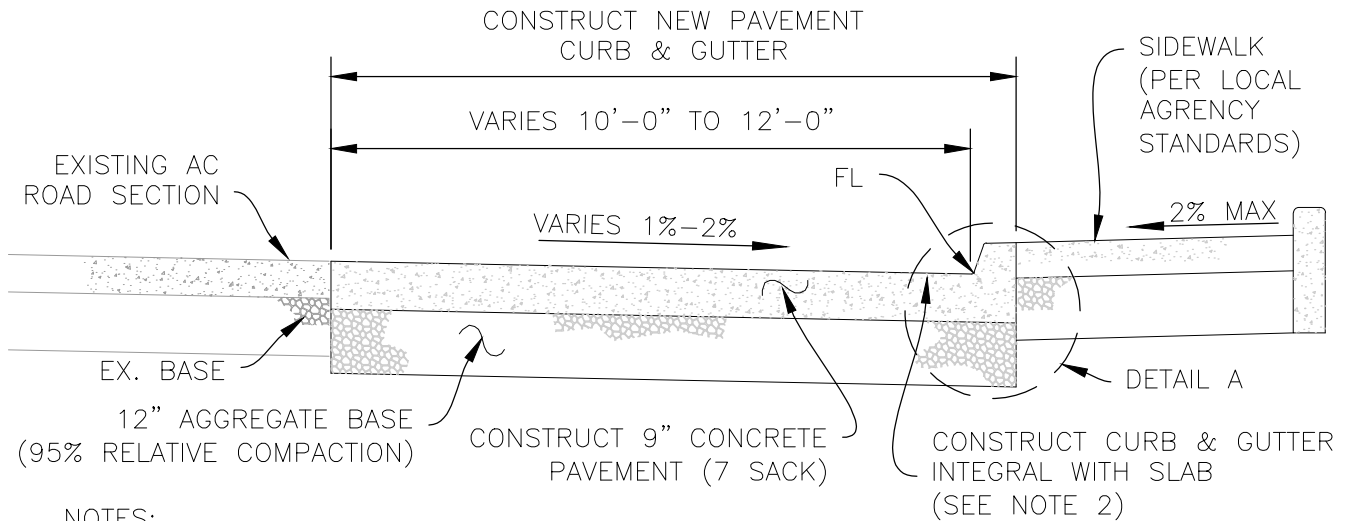
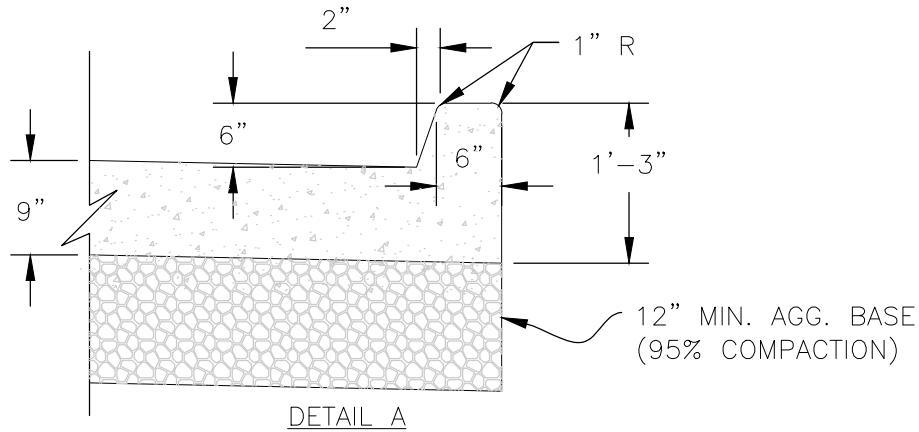
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**FIGURE No.7-3 (c)  
BUS STOP SIGNAGE  
AT TRANSIT CENTERS**



**NOTES:**

1. WHERE LOCAL AGENCY STANDARDS FOR BUS STOPS ARE NOT APPLICABLE CURB & GUTTER SHALL BE PER DETAIL A. CURBS SHALL NOT BE MOUNTABLE TYPE.
2. CURB AND GUTTER SHALL BE CONSTRUCTED INTEGRAL AS MONOLITHIC POUR WHERE THIS IS NOT POSSIBLE CONCRETE PAVEMENT AND GUTTER SHALL BE DOWELED PER STANDARD PLAN A-35-B FOR LONGITUDINAL JOINTS
3. FOR DETAILS REGARDING JOINT SPACING, DOWELS, TIE BARS AND WEAKENED PLANE JOINTS SEE LATEST APPROVED CALTRANS STANDARD PLANS.
4. JOINTS SHALL BE SEALED WITH POURED SEALANT AS APPROVED BY THE ENGINEER.
5. WEAKENED PLANE DOWELED TRANSVERSE JOINT SHALL BE PER LOCAL AGENCY STANDARDS AND SHALL BE CONSTRUCTED AT 12 FEET MAX SPACING.
6. WHERE THE IMPROVEMENTS LIE WITHIN THE JURISDICTION OF LOCAL AGENCY THAT MAINTAINS DESIGN STANDARDS THAT EXCEED THESE STANDARDS, THEN THE LOCAL AGENCY STANDARDS SHALL APPLY.



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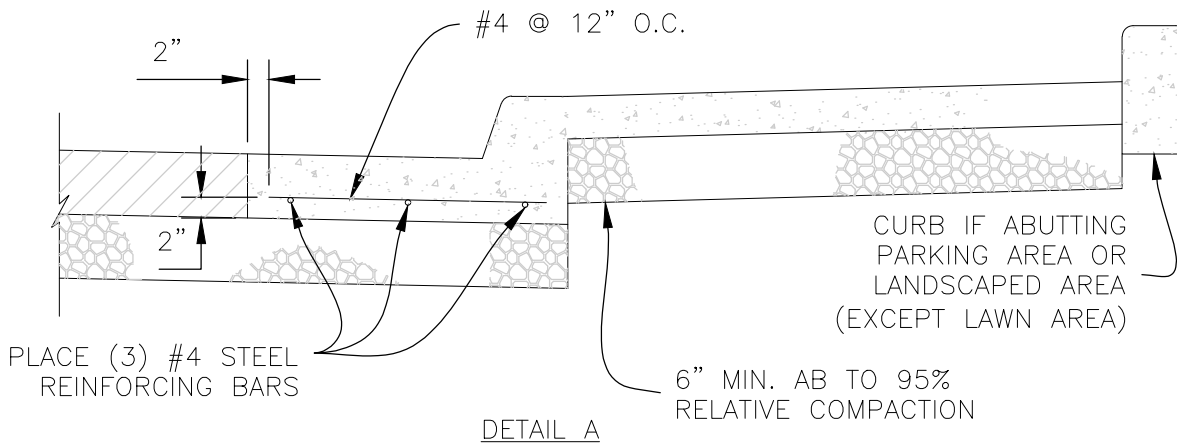
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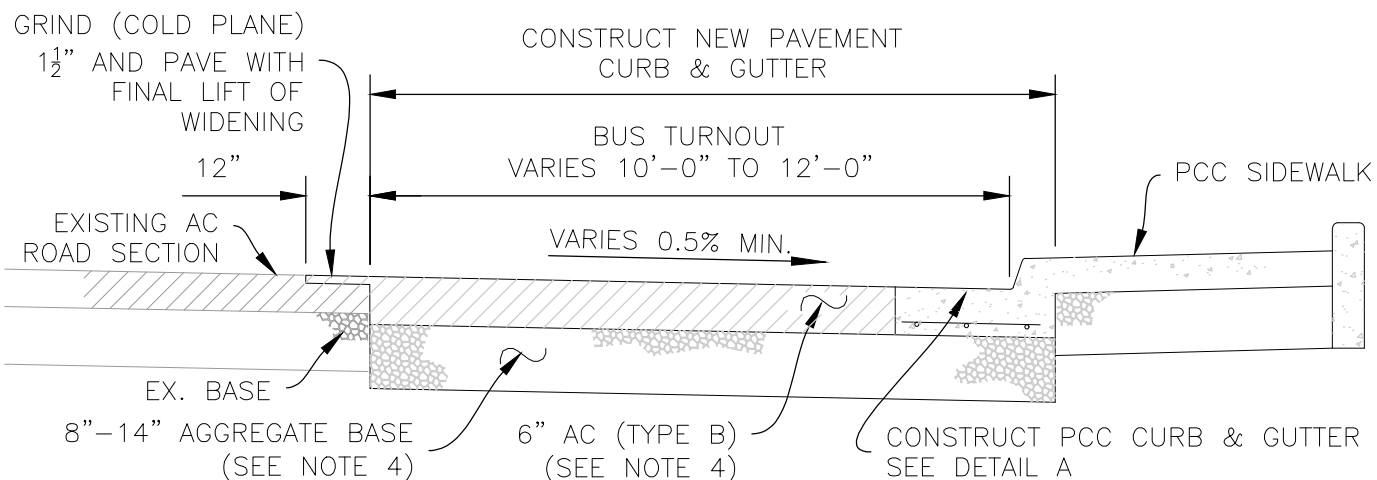
**FIGURE No.7-4**

**PAVEMENT AND CURB DETAIL FOR  
BUS STOP (HIGH TRAFFIC VOLUME)**



**NOTES:**

1. DETAIL 'A' SECTION SHALL BE USED FOR 100 FEET EACH SIDE OF BUS STOPS WITHOUT TURNOUTS. THE REINFORCING STEEL SHALL BE CONTINUED ACROSS ANY DRIVEWAYS WITHIN THE 100 FOOT DISTANCE FROM THE BUS STOP.
2. SEE FIGURE No.4-1 FOR BUS STOP AND BUS STOP TURNOUT DETAILS.
3. TRANSITION LENGTH FROM REINFORCED DETAIL 'A' TO ABUTTING CURB AND GUTTER SECTION SHALL BE 5' BEYOND LIMITS OF BUS STOP AREA.
4. THICKNESS DEPENDENT ON SUBGRADE SUPPORT.
5. PCC CURB & GUTTER AND SIDEWALK SHALL BE MONOLITHIC POUR.
6. FOR CURB & GUTTER DETAILS NOT SHOWN SEE DETAIL A (FIGURE 3-3).
7. WHERE THE IMPROVEMENTS LIE WITHIN THE JURISDICTION OF A LOCAL AGENCY THAT MAINTAINS DESIGN STANDARDS THAT EXCEED THESE STANDARDS, THEN THE LOCAL AGENCY STANDARDS SHALL APPLY.



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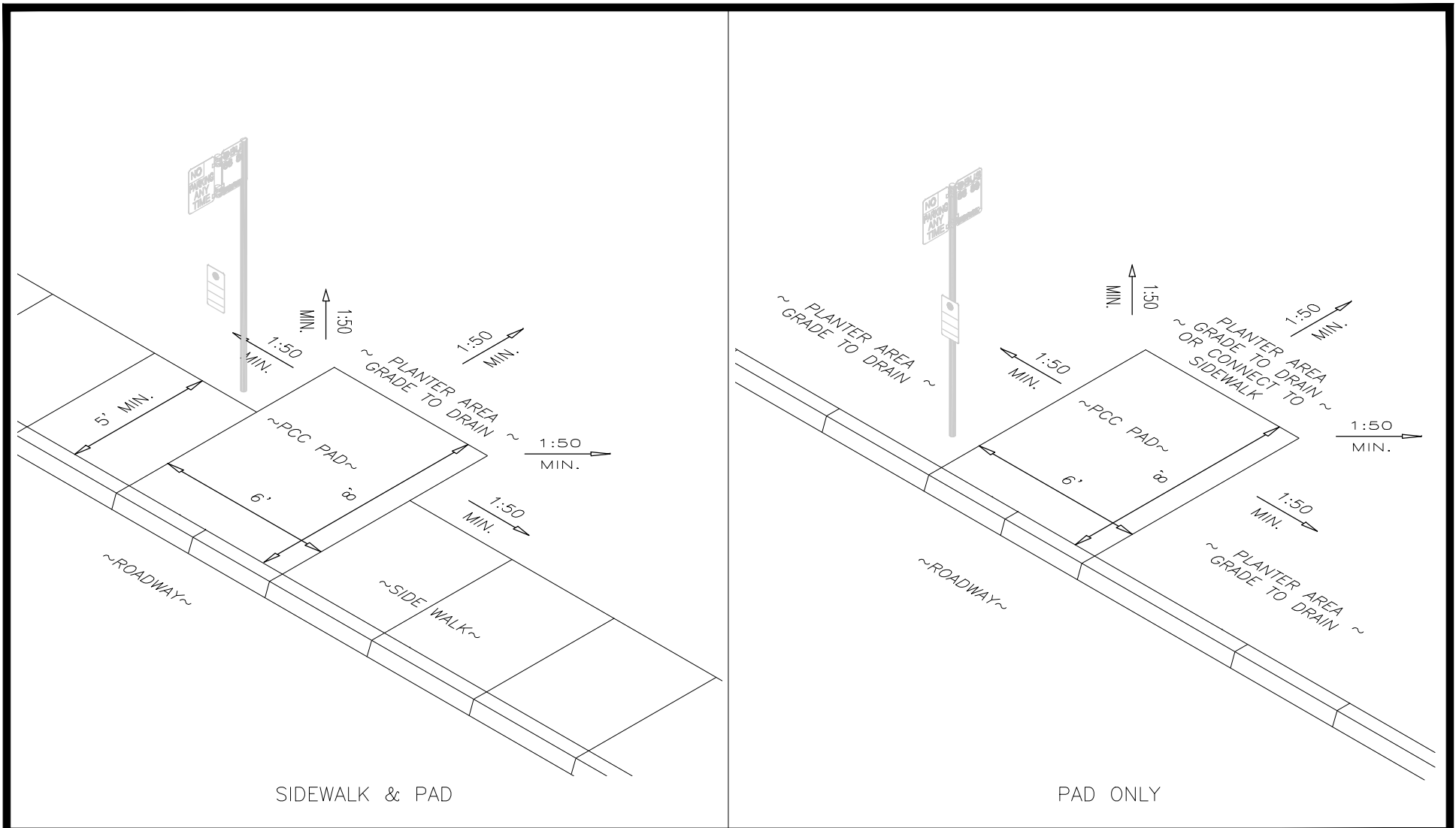
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**FIGURE No. 7-5**

**PAVEMENT AND CURB DETAIL FOR  
BUS STOP (LOW TRAFFIC VOLUME)**



SIDEWALK & PAD

PAD ONLY



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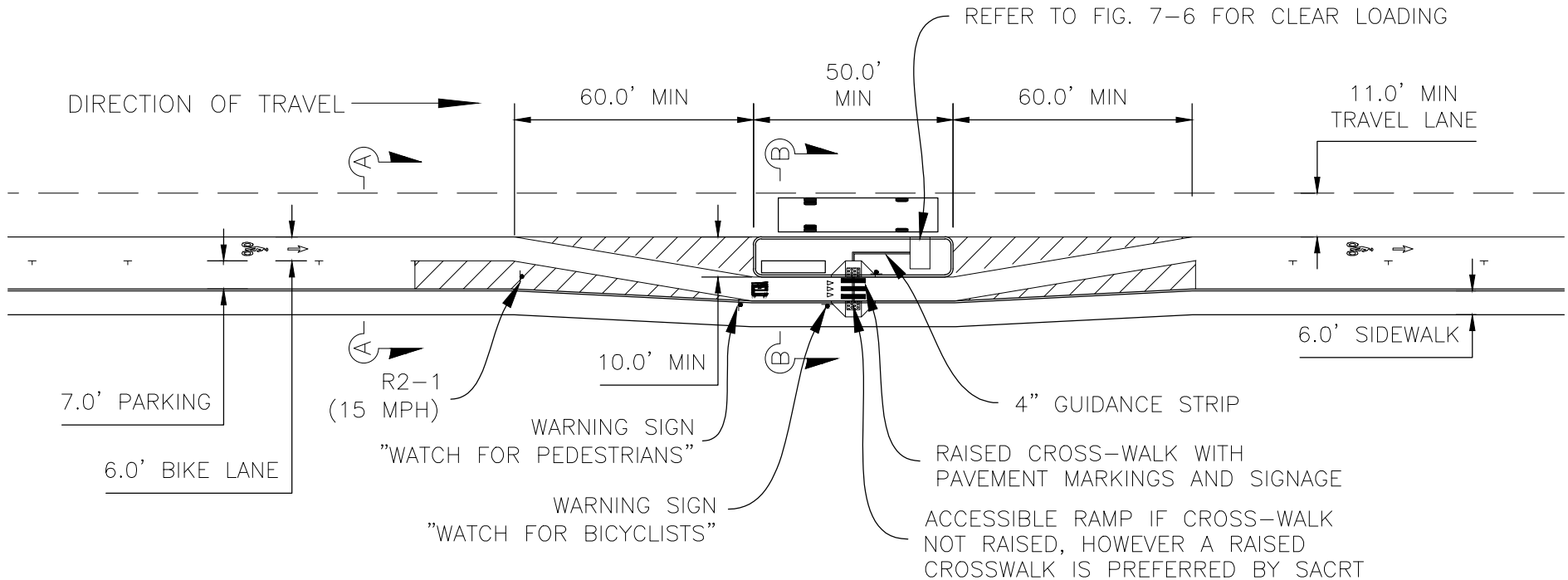
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**FIGURE NO. 7-6  
BUS STOP PAD  
NO SHELTER OR BENCH**



NOTES:

1. UNLESS OTHERWISE NOTED THE DIMENSIONS SHOWN ARE MINIMUM STANDARDS.
2. WHERE THE BUS TURNOUT LIES WITHIN THE JURISDICTION OF A LOCAL AGENCY THAT MAINTAINS DESIGN STANDARDS THAT EXCEED THESE STANDARDS, THEN THE LOCAL AGENCY STANDARDS APPLY.
3. WHERE HIGH PEDESTRIAN AND PASSENGER VOLUMES OCCUR SIDEWALK MUST BE 8 FEET.
4. ALL SIGNAGE, STRIPING AND PAVEMENT MARKINGS MUST BE IN ACCORDANCE WITH CALIFORNIA MUTCD.



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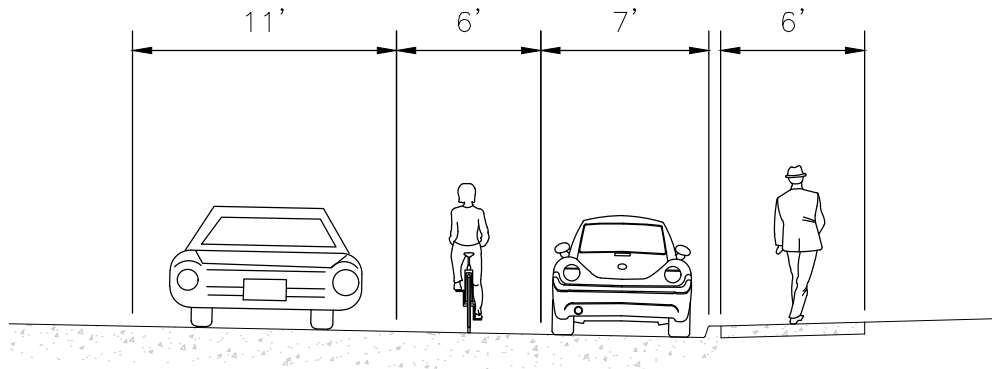
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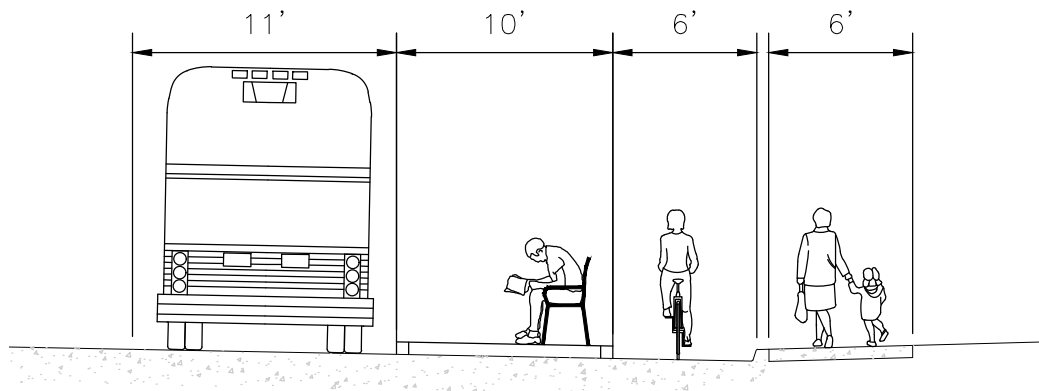
**FIGURE No. 7-7 (a)**

**IN-LANE MID-BLOCK BUS STOP**

**WITH CLASS II BIKE LANE & FLOATING TRANSIT ISLAND**



SECTION A-A



SECTION B-B



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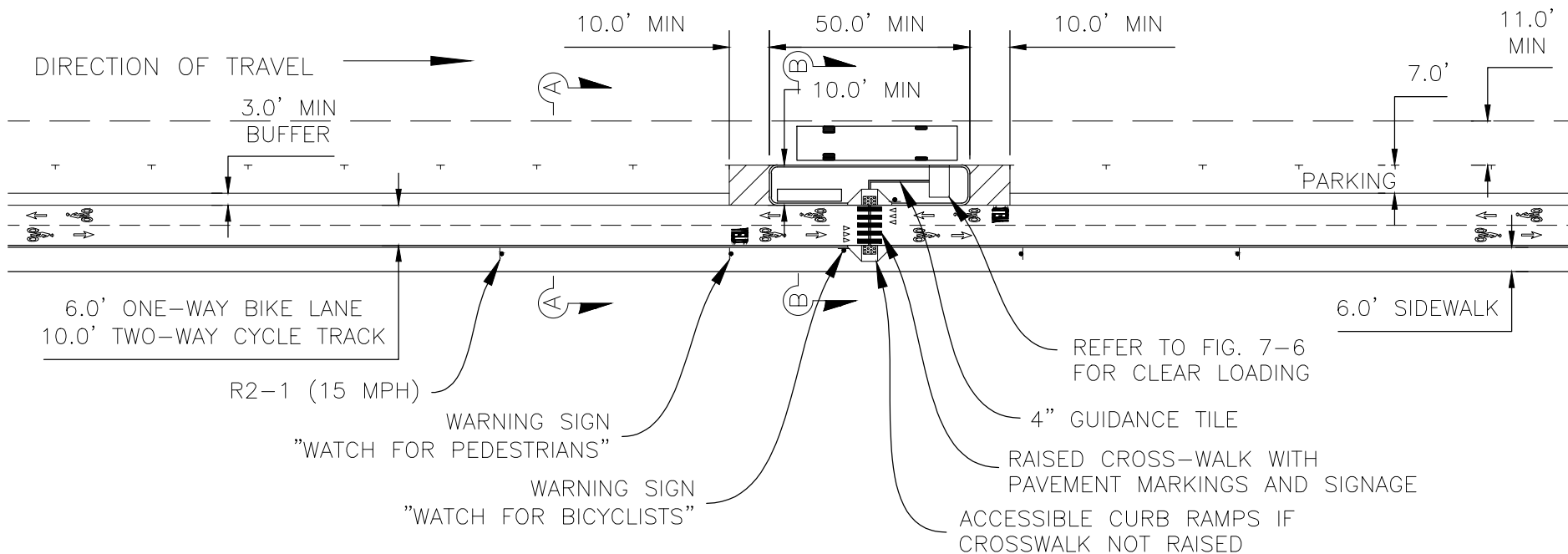
APPROVED:

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FIGURE No.7-7 (b)

**IN-LANE MID-BLOCK BUS STOP  
CLASS II BIKE LANE & FLOATING TRANSIT ISLAND**



**NOTES:**

1. UNLESS OTHERWISE NOTED THE DIMENSIONS SHOWN ARE MINIMUM STANDARDS.
2. WHERE THE BUS TURNOUT LIES WITHIN THE JURISDICTION OF A LOCAL AGENCY THAT MAINTAINS DESIGN STANDARDS THAT EXCEED THESE STANDARDS, THEN THE LOCAL AGENCY STANDARDS APPLY.
3. WHERE HIGH PEDESTRIAN AND PASSENGER VOLUMES OCCUR SIDEWALK MUST BE 8 FEET.
4. ALL SIGNAGE, STRIPING AND PAVEMENT MARKINGS MUST BE IN ACCORDANCE WITH THE CALIFORNIA MUTCD.



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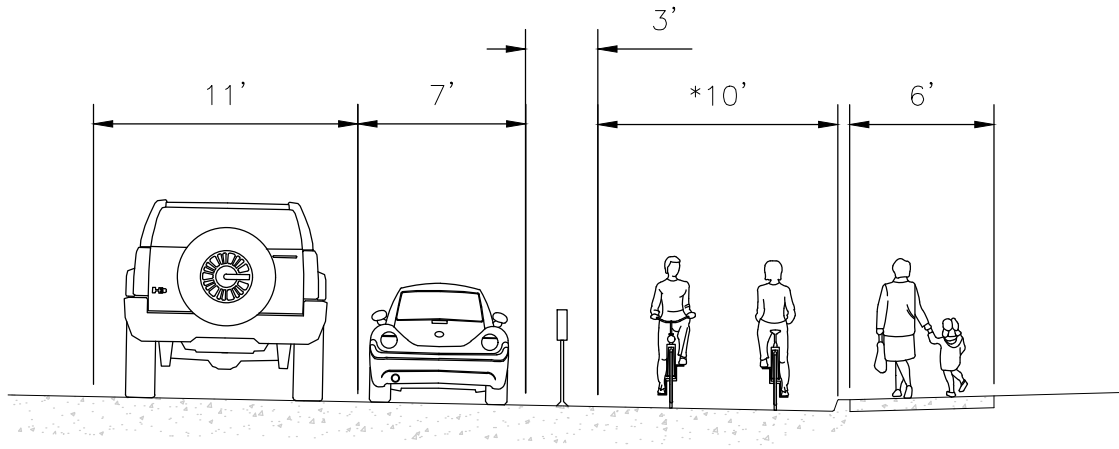
**FIGURE No. 7-8 (a)**

**MID-BLOCK IN-LANE BUS STOP  
WITH CLASS IV BIKEWAY & FLOATING TRANSIT ISLAND**

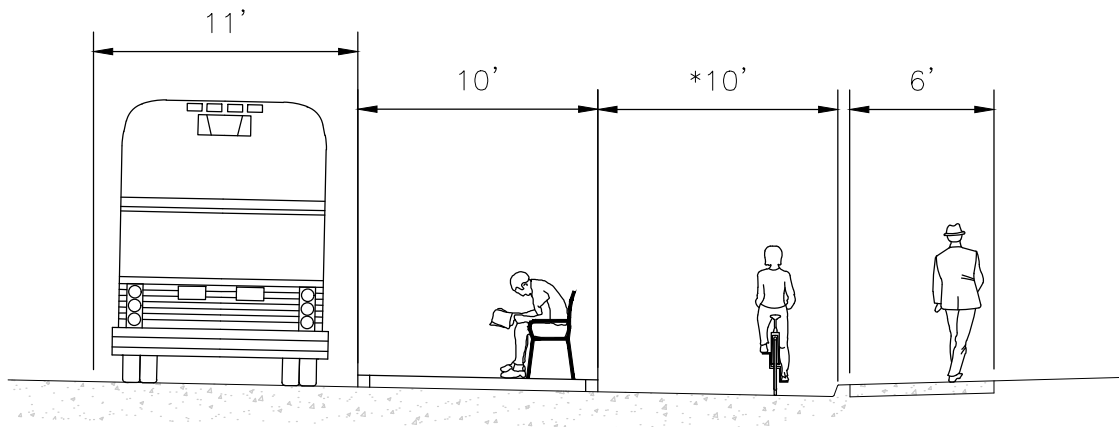


NOTES:

- \* 6.0' ONE-WAY BIKE LANE WITH 3.0' MIN BUFFER. 10.0' TWO-WAY CYCLE TRACK WITH MIN 3.0' BUFFER.



SECTION A-A



SECTION B-B



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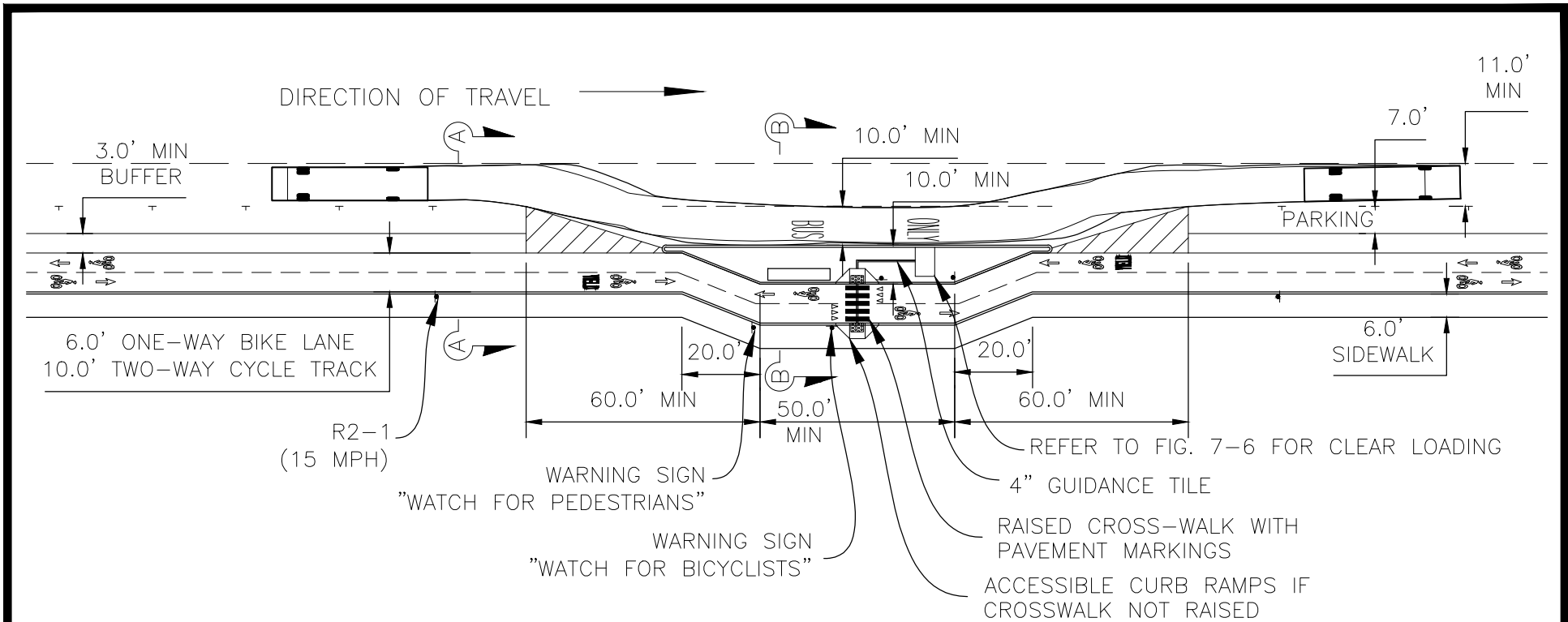
APPROVED:

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DATE:

FIGURE No. 7-8 (b)

**MID-BLOCK IN-LANE BUS STOP  
CLASS IV BIKEWAY & FLOATING TRANSIT ISLAND**



**NOTES:**

1. UNLESS OTHERWISE NOTED THE DIMENSIONS SHOWN ARE MINIMUM STANDARDS.
2. WHERE THE BUS TURNOUT LIES WITHIN THE JURISDICTION OF A LOCAL AGENCY THAT MAINTAINS DESIGN STANDARDS THAT EXCEED THESE STANDARDS, THEN THE LOCAL AGENCY STANDARDS APPLY.
3. WHERE HIGH PEDESTRIAN AND PASSENGER VOLUMES OCCUR SIDEWALK MUST BE 8 FEET.
4. ALL SIGNAGE, STRIPING AND PAVEMENT MARKINGS MUST BE IN ACCORDANCE WITH CALIFORNIA MUTCD.



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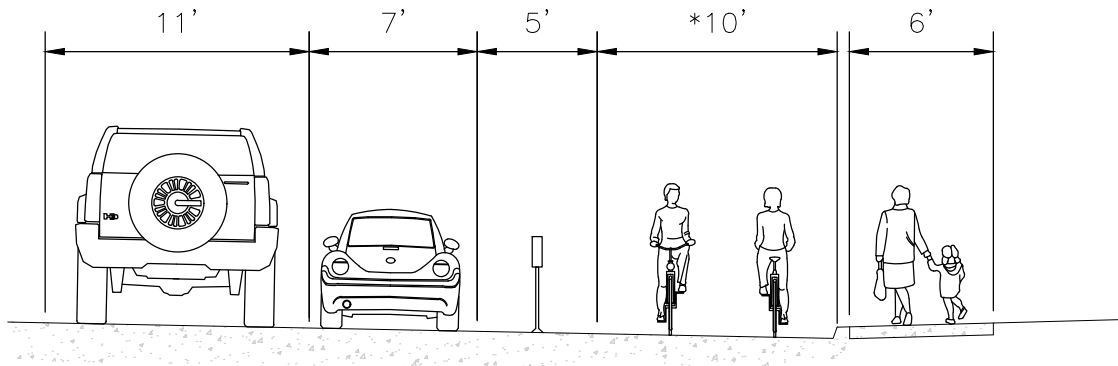
**FIGURE No. 7-9 (a)**

**MID-BLOCK BUS ONLY LANE BUS STOP  
WITH CLASS IV BIKEWAY & FLOATING TRANSIT ISLAND**

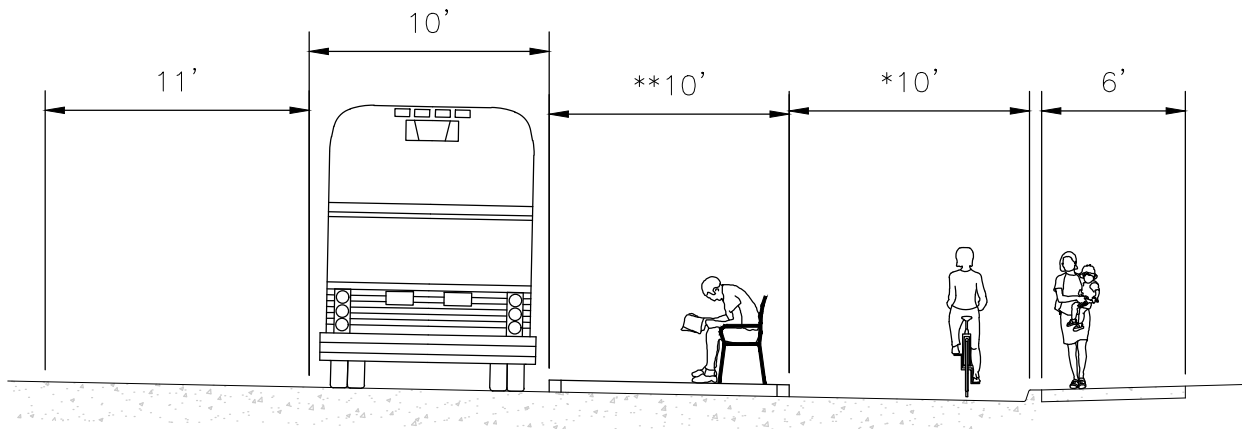
NOTES:

\* 6.0' ONE-WAY BIKE LANE WITH 3.0' MIN BUFFER. 10.0' TWO-WAY CYCLE TRACK WITH MIN 3.0' BUFFER. 5.0' BUFFER SHOWN.

\*\* 10.0' WIDE MINIMUM ISLAND WIDTH.



SECTION A-A



SECTION B-B



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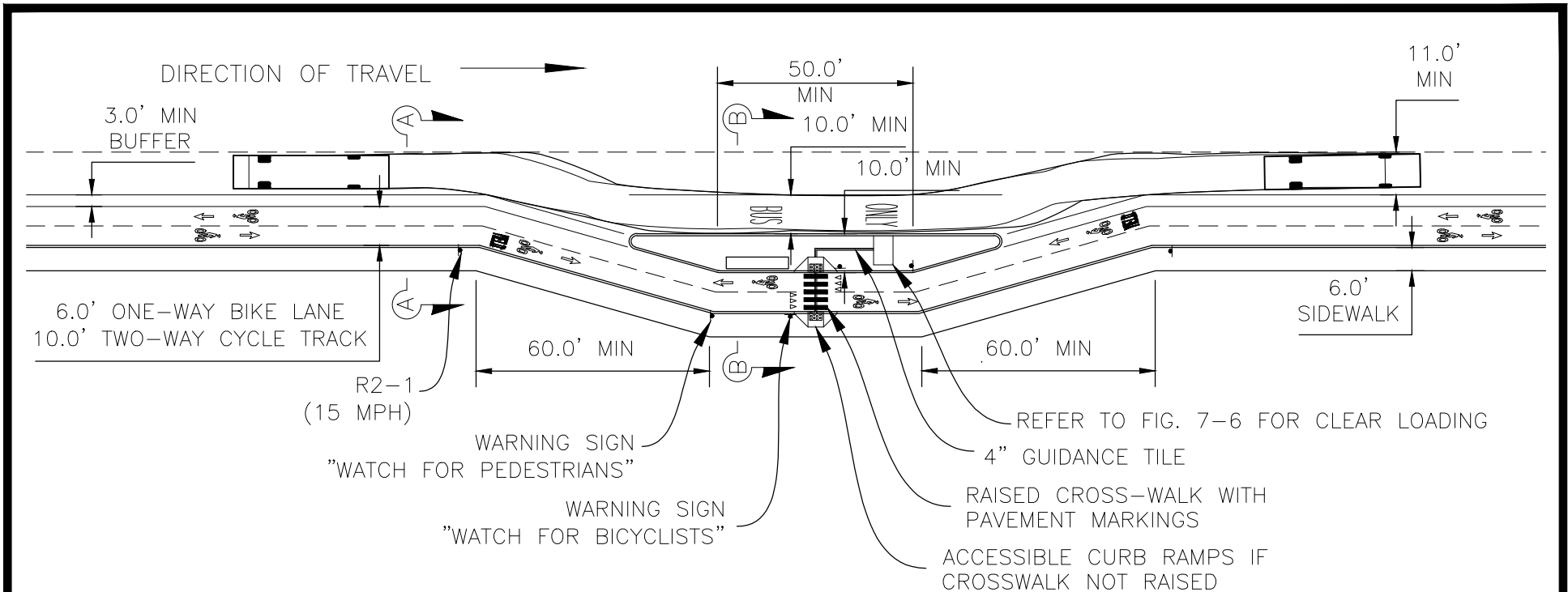
## DESIGN GUIDELINES

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FIGURE No. 7-9 (b)  
**CLASS IV BIKE LANE  
BUS ONLY LANE - FLOATING STOP**



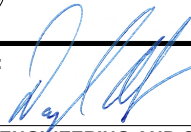
**NOTES:**

1. UNLESS OTHERWISE NOTED THE DIMENSIONS SHOWN ARE MINIMUM STANDARDS.
2. WHERE THE BUS TURNOUT LIES WITHIN THE JURISDICTION OF A LOCAL AGENCY THAT MAINTAINS DESIGN STANDARDS THAT EXCEED THESE STANDARDS, THEN THE LOCAL AGENCY STANDARDS APPLY.
3. WHERE HIGH PEDESTRIAN AND PASSENGER VOLUMES OCCUR SIDEWALK MUST BE 8 FEET.
4. ALL SIGNAGE, STRIPING AND PAVEMENT MARKINGS MUST BE IN ACCORDANCE WITH CALIFORNIA MUTCD.



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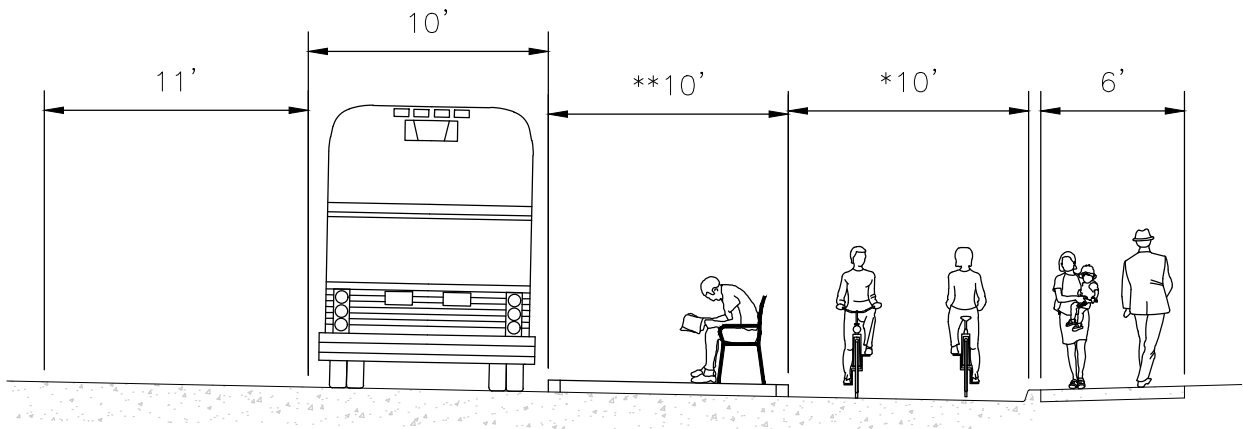
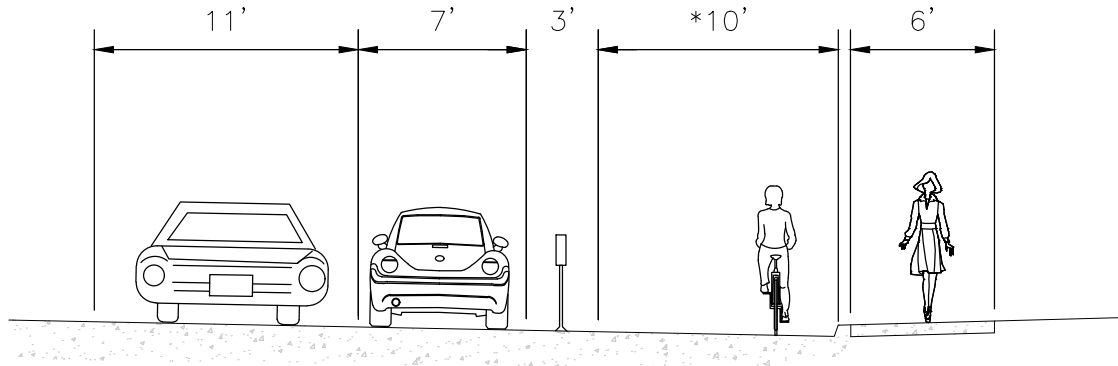
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**FIGURE No. 7-10 (a)**  
**MID-BLOCK BUS ONLY LANE BUS STOP**  
**WITH CLASS IV BIKEWAY ONLY & FLOATING TRANSIT ISLAND**

NOTES:

\* 6.0' ONE-WAY BIKE LANE WITH 3.0' MIN BUFFER. 10.0' TWO-WAY CYCLE TRACK WITH MIN 3.0' BUFFER.

\*\* 10.0' WIDE MINIMUM ISLAND WIDTH.



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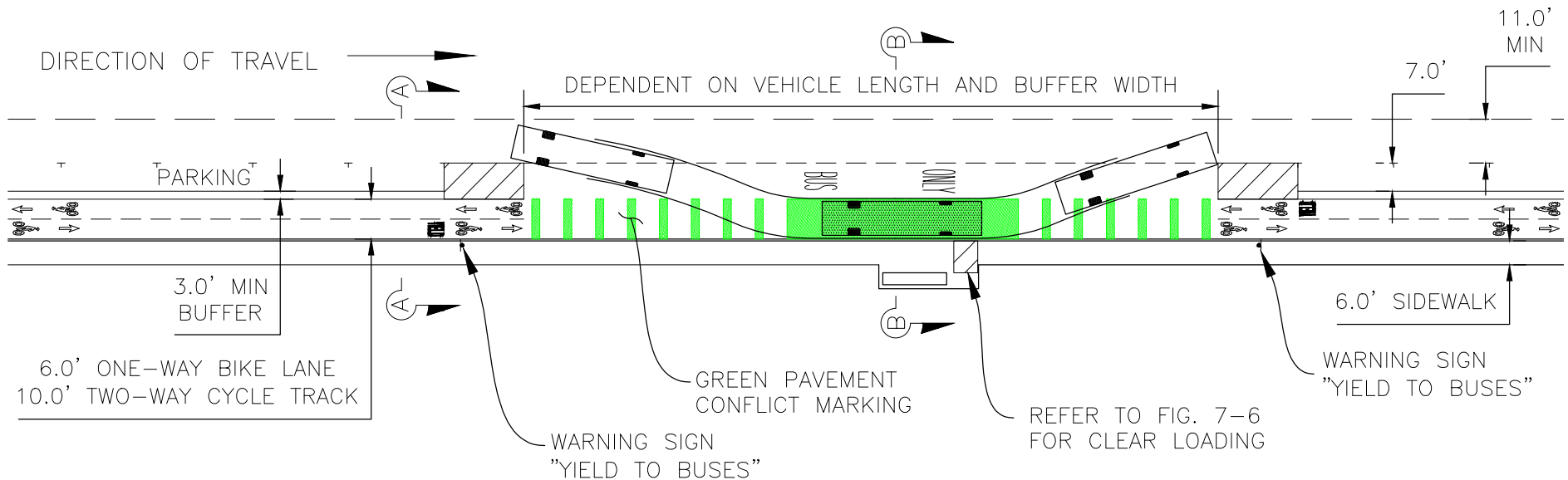
## DESIGN GUIDELINES

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FIGURE No. 7-10 (b)  
**CLASS IV BIKE LANE  
BUS ONLY LANE - FLOATING STOP**



**NOTES:**

1. UNLESS OTHERWISE NOTED THE DIMENSIONS SHOWN ARE MINIMUM STANDARDS.
2. WHERE THE BUS TURNOUT LIES WITHIN THE JURISDICTION OF A LOCAL AGENCY THAT MAINTAINS DESIGN STANDARDS THAT EXCEED THESE STANDARDS, THEN THE LOCAL AGENCY STANDARDS APPLY.
3. WHERE HIGH PEDESTRIAN AND PASSENGER VOLUMES OCCUR SIDEWALK MUST BE 8 FEET.
4. ALL SIGNAGE, STRIPING AND PAVEMENT MARKINGS MUST BE IN ACCORDANCE WITH CALIFORNIA MUTCD.



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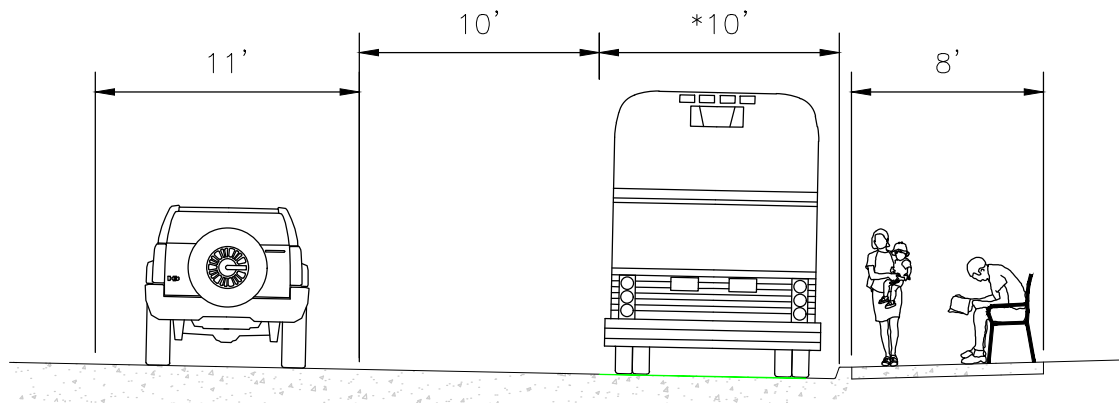
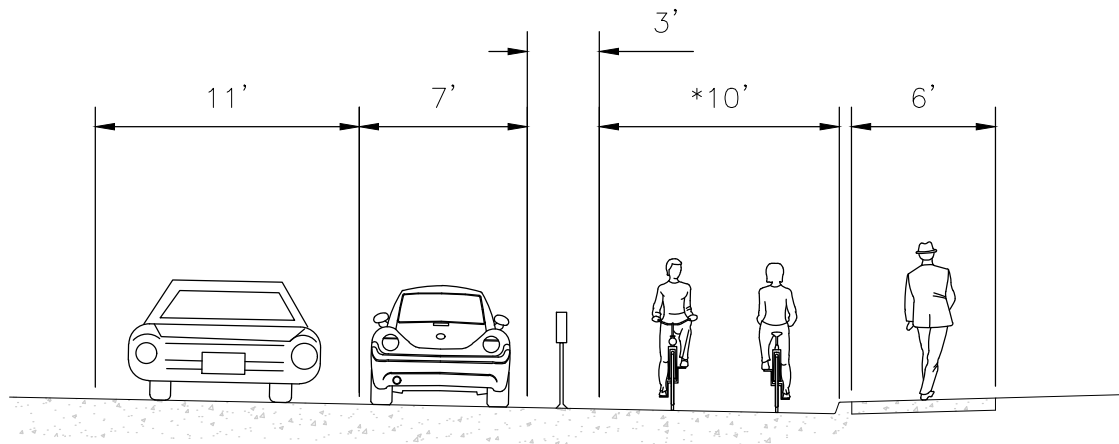
DATE:

**FIGURE No. 7-11 (a)**

**MID-BLOCK CURBSIDE BUS STOP  
WITH CLASS IV BIKEWAY (CYCLE TRACK)**

NOTES:

- \* 6.0' ONE-WAY BIKE LANE WITH 3.0' MIN BUFFER. 10.0' TWO-WAY CYCLE TRACK WITH MIN 3.0' BUFFER.



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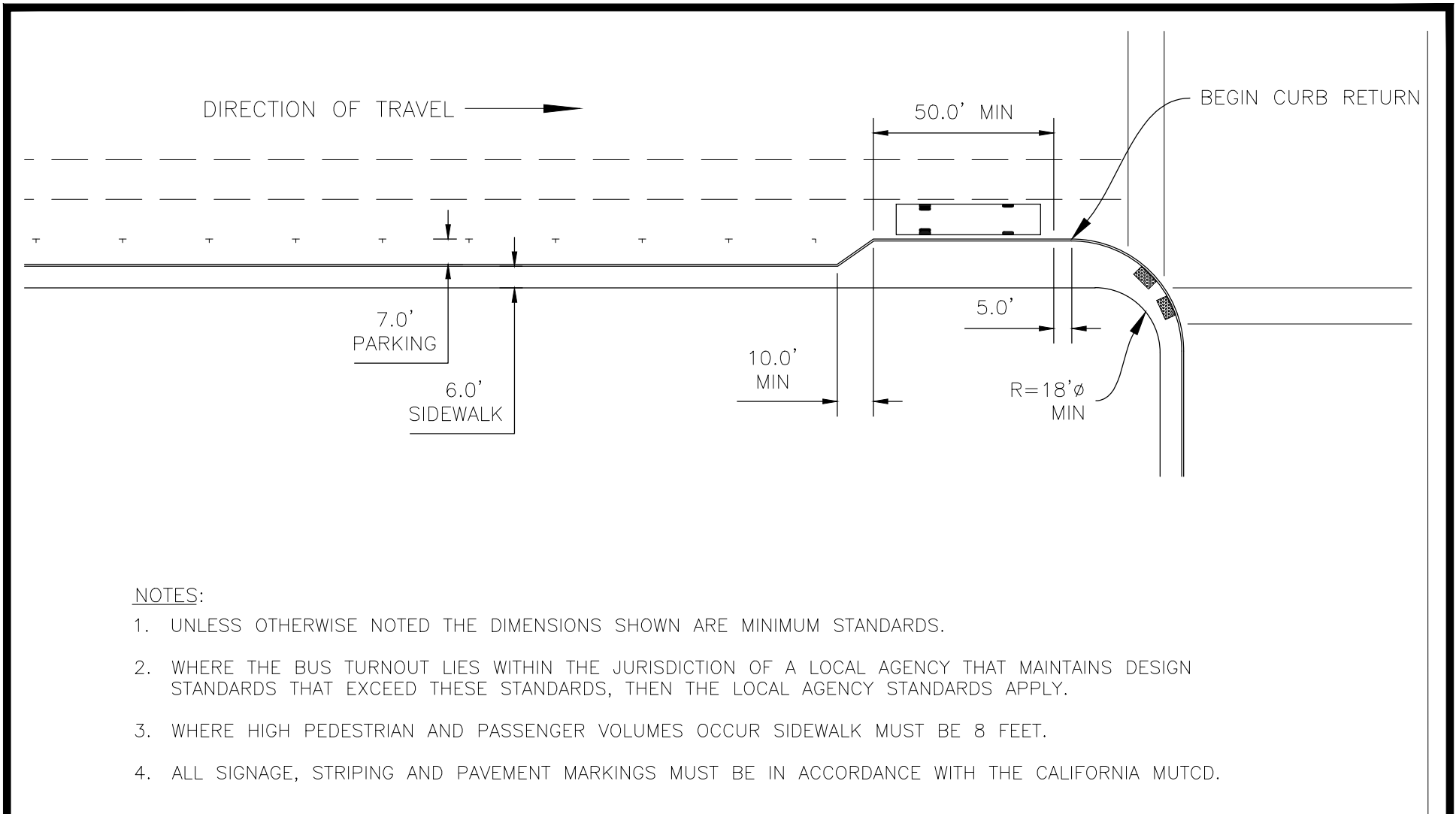
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FIGURE No. 7-11 (b)

**MID-BLOCK CURBSIDE BUS STOP  
CLASS IV BIKEWAY (CYCLE TRACK)**



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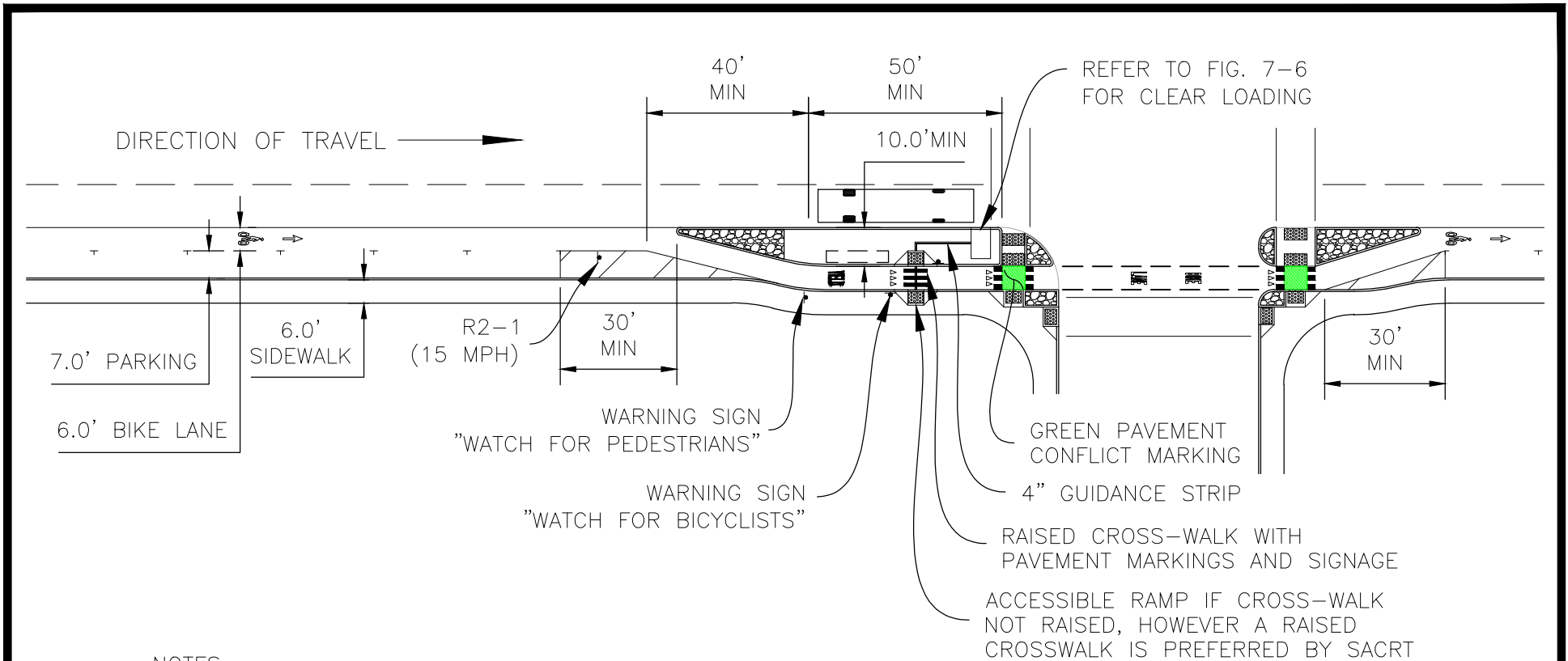
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**FIGURE No. 7-12**

**NEAR-SIDE IN-LANE BUS STOP  
BULB-OUT**





NOTES:

1. UNLESS OTHERWISE NOTED THE DIMENSIONS SHOWN ARE MINIMUM STANDARDS.
2. WHERE THE BUS TURNOUT LIES WITHIN THE JURISDICTION OF A LOCAL AGENCY THAT MAINTAINS DESIGN STANDARDS THAT EXCEED THESE STANDARDS, THEN THE LOCAL AGENCY STANDARDS APPLY.
3. WHERE HIGH PEDESTRIAN AND PASSENGER VOLUMES OCCUR SIDEWALK MUST BE 8 FEET.
4. ALL SIGNAGE, STRIPING AND PAVEMENT MARKINGS MUST BE IN ACCORDANCE WITH CALIFORNIA MUTCD.

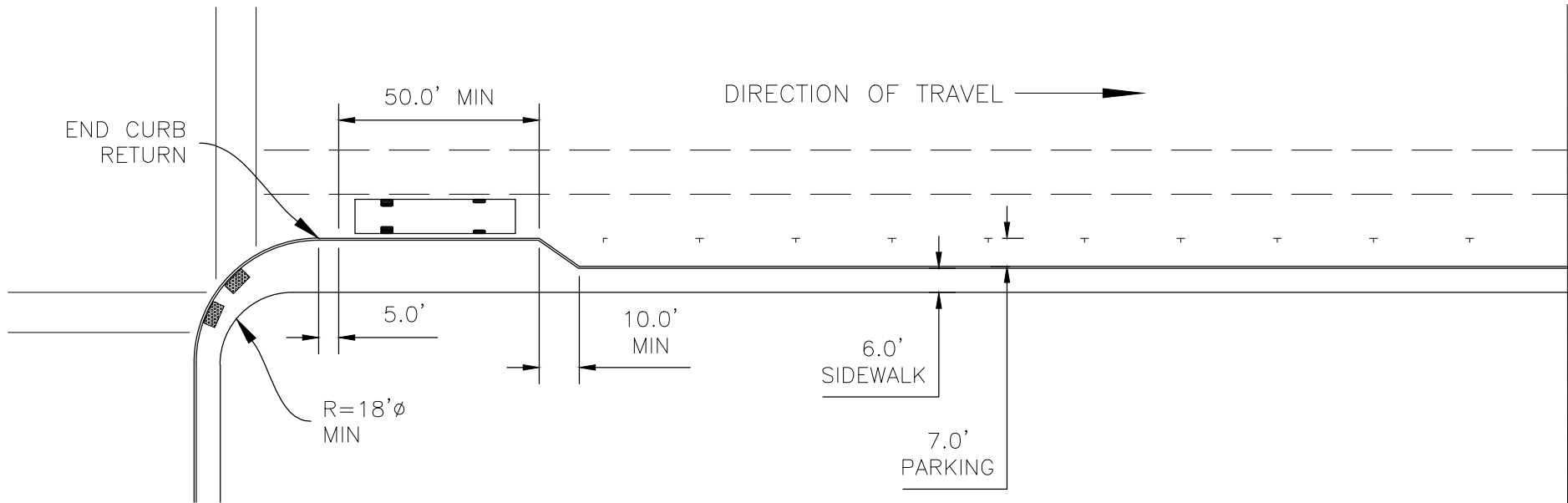


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**FIGURE No. 7-13**  
**NEAR SIDE IN-LANE BUS STOP**  
**WITH CLASS II BIKE LANE & FLOATING TRANSIT ISLAND**



**NOTES:**

1. UNLESS OTHERWISE NOTED THE DIMENSIONS SHOWN ARE MINIMUM STANDARDS.
2. WHERE THE BUS TURNOUT LIES WITHIN THE JURISDICTION OF A LOCAL AGENCY THAT MAINTAINS DESIGN STANDARDS THAT EXCEED THESE STANDARDS, THEN THE LOCAL AGENCY STANDARDS APPLY.
3. WHERE HIGH PEDESTRIAN AND PASSENGER VOLUMES OCCUR SIDEWALK MUST BE 8 FEET.
4. ALL SIGNAGE, STRIPING AND PAVEMENT MARKINGS MUST BE IN ACCORDANCE WITH THE CALIFORNIA MUTCD.



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**DESIGN GUIDELINES**

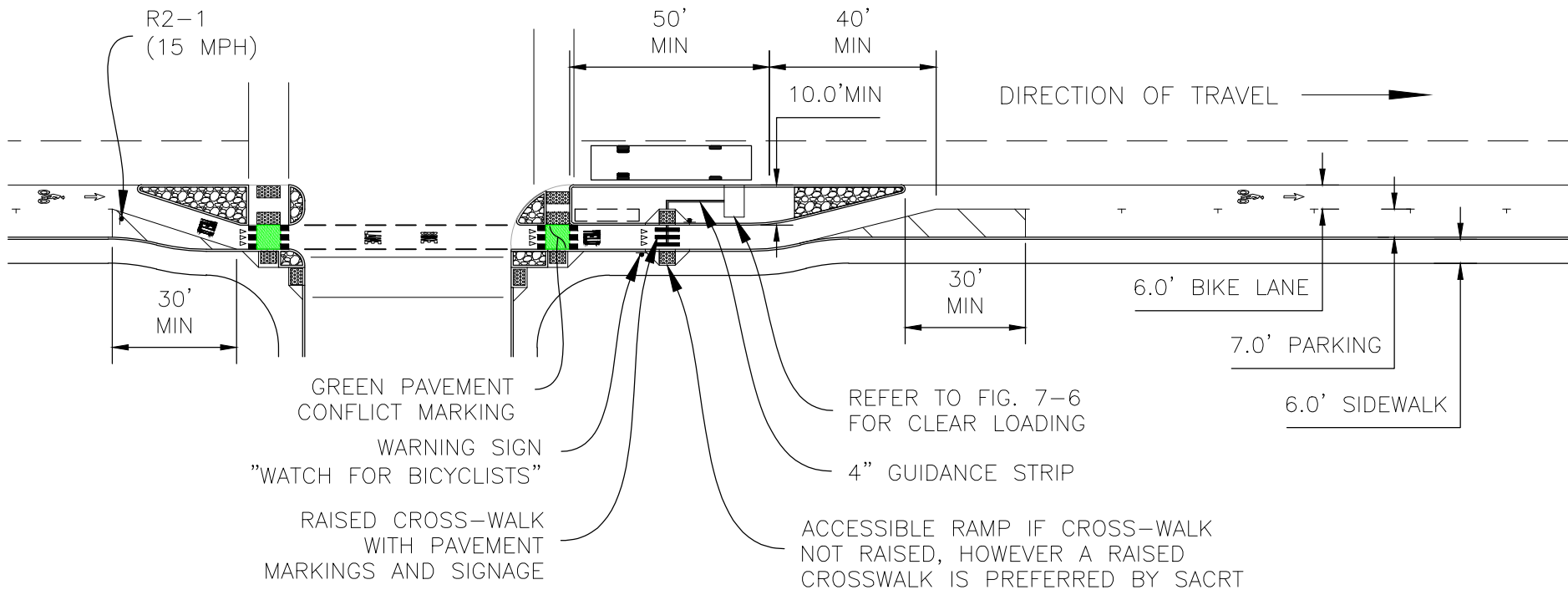
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**FIGURE No. 7-14**

**FAR SIDE IN-LANE BUS STOP  
BULB-OUT**



NOTES:

1. UNLESS OTHERWISE NOTED THE DIMENSIONS SHOWN ARE MINIMUM STANDARDS.
2. WHERE THE BUS TURNOUT LIES WITHIN THE JURISDICTION OF A LOCAL AGENCY THAT MAINTAINS DESIGN STANDARDS THAT EXCEED THESE STANDARDS, THEN THE LOCAL AGENCY STANDARDS APPLY.
3. WHERE HIGH PEDESTRIAN AND PASSENGER VOLUMES OCCUR SIDEWALK MUST BE 8 FEET.
4. ALL SIGNAGE, STRIPING AND PAVEMENT MARKINGS MUST BE IN ACCORDANCE WITH CALIFORNIA MUTCD.



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**DESIGN GUIDELINES**

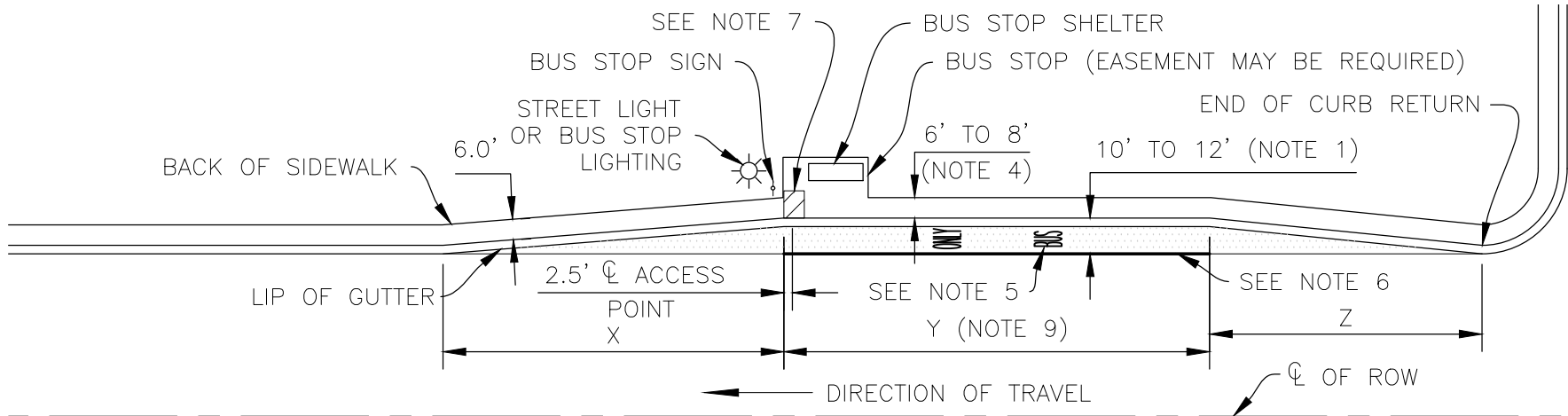
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**FIGURE No. 7-15**

**FAR SIDE IN-LANE BUS STOP  
WITH CLASS II BIKE LANE & FLOATING TRANSIT ISLAND**



**NOTES:**

1. 10' TO 12' WIDE BUS PAD – P.C.C. PAVEMENT, MEASURED FROM FACE OF CURB. IF BIKE LANE IS LOCATED WITHIN THE TURNOUT AREA WIDTH MUST BE INCREASED TO 12 FEET (OR THE WIDTH OF THE BIKE LANE). FOR FACILITIES WITH NO BIKE LANE, WIDTH SHALL BE 10 FEET.
2. UNLESS OTHERWISE NOTED THE DIMENSIONS SHOWN ARE MINIMUM STANDARDS.
3. WHERE THE LOCAL AGENCY HAVING JURISDICTION OVER THE FACILITY MAINTAIN DESIGN STANDARDS THAT EXCEED THE STANDARDS SHOWN HERE, THE LOCAL AGENCY STANDARDS SHALL APPLY.
4. WHERE HIGH PEDESTRIAN AND PASSENGER VOLUMES OCCUR SIDEWALK SHALL BE 8 FEET.
5. "BUS ONLY" LEGEND PER CALTRANS STANDARD PLANS A-24-E

6. WHITE STRIPE PER CALTRANS STANDARD PLANS A-20-D, DETAIL 38A (OPTIONAL)
7. REFER TO FIGURE NO. 7-6 FOR FOR BUS STOP PAD MINIMUM CLEAR DIMENSIONS.
8. DRIVEWAYS AND CURB RAMPS SHOULD BE AVOIDED WITHIN TURNOUT/BUS ZONE.
9. WHERE MULTIPLE BUSES ARE SCHEDULED AT ONE STOP, AN ADDITIONAL 80 FEET SHALL BE ADDED TO LENGTH "Y" PER BUS.

FACILITY TYPE	DESIGN SPEED (mph)	MINIMUM			PREFERRED		
		X	Y	Z	X	Y	Z
COLLECTOR	25-40	65'	125'	45'	#'	#'	#'
ARTERIAL	40-50	100'	125'	65'	#'	#'	#'
THOROUGHFARE	45-55	100'	125'	85'	#'	#'	#'



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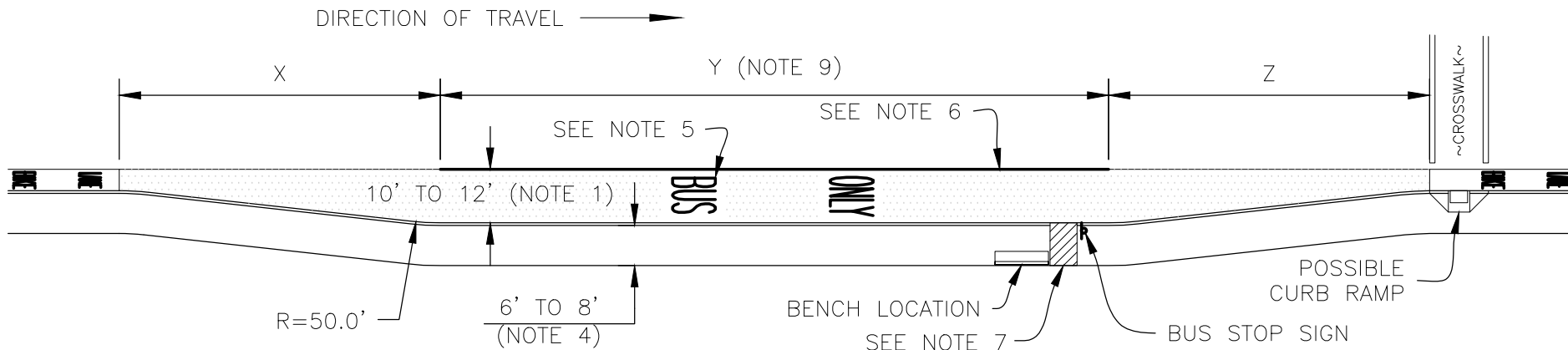
**DESIGN GUIDELINES**

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**FIGURE No. 8-1  
FAR SIDE BUS TURNOUT**



**NOTES:**

- 10' TO 12' WIDE BUS PAD – P.C.C. PAVEMENT, MEASURED FROM FACE OF CURB. IF BIKE LANE IS LOCATED WITHIN THE TURNOUT AREA WIDTH MUST BE INCREASED TO 12 FEET (OR THE WIDTH OF THE BIKE LANE). FOR FACILITIES WITH NO BIKE LANE, WIDTH SHALL BE 10 FEET.
- UNLESS OTHERWISE NOTED THE DIMENSIONS SHOWN ARE MINIMUM STANDARDS.
- WHERE THE LOCAL AGENCY HAVING JURISDICTION OVER THE FACILITY MAINTAIN DESIGN STANDARDS THAT EXCEED THE STANDARDS SHOWN HERE, THE LOCAL AGENCY STANDARDS SHALL APPLY.
- WHERE HIGH PEDESTRIAN AND PASSENGER VOLUMES OCCUR SIDEWALK SHALL BE 8 FEET.
- "BUS ONLY" LEGEND PER CALTRANS STANDARD PLANS A-24-E
- WHITE STRIPE PER CALTRANS STANDARD PLANS A-20-D, DETAIL 38A (OPTIONAL)
- REFER TO FIGURE NO. 7-6 FOR FOR BUS STOP PAD MINIMUM CLEAR DIMENSIONS.
- DRIVEWAYS AND CURB RAMPS SHOULD BE AVOIDED WITHIN TURNOUT/BUS ZONE.
- WHERE MULTIPLE BUSES ARE SCHEDULED AT ONE STOP, AN ADDITIONAL 80 FEET SHALL BE ADDED TO LENGTH "Y" PER BUS.
- WHERE THE BUS PULLOUT IS LOCATED WITHIN RT FACILITY THIS DIMENSION MAY BE REDUCED TO 60 FEET FOR EACH BUS PER HOUR.

FACILITY TYPE	DESIGN SPEED (mph)	MINIMUM			PREFERRED		
		X	Y	Z	X	Y	Z
COLLECTOR	25-40	45'	125'	45'	#'	#'	#'
ARTERIAL	40-50	60'	125'	60'	#'	#'	#'
THOROUGHFARE	45-55	80'	125'	80'	#'	#'	#'



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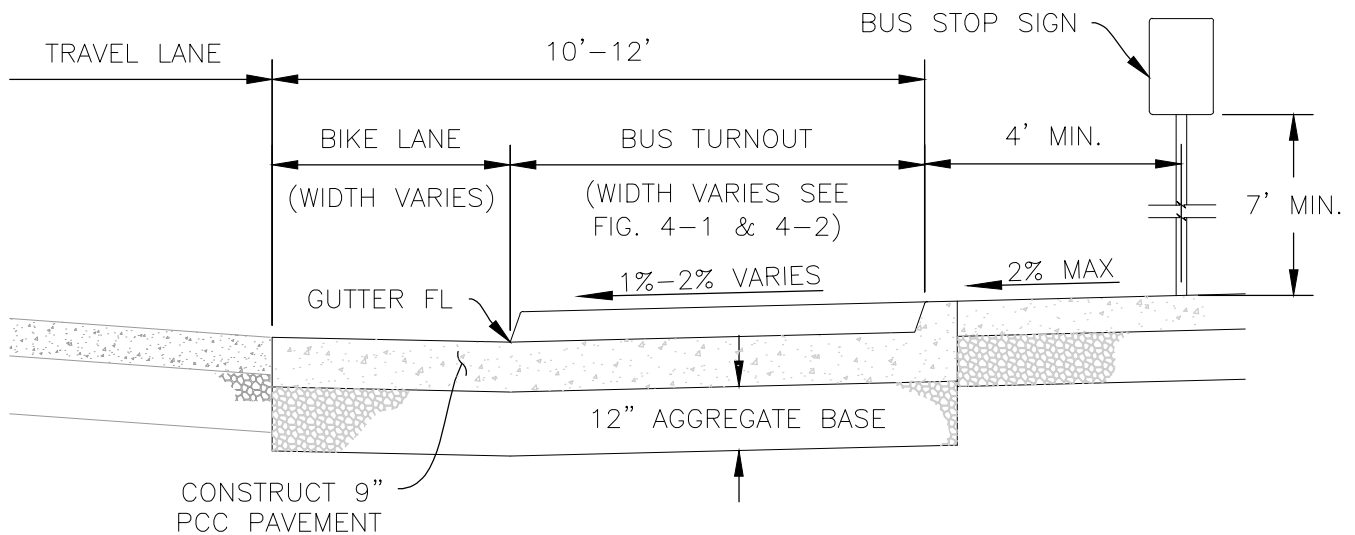
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**FIGURE No. 8-2**

**MID BLOCK BUS TURNOUT**



NOTES:

1. CURB & GUTTER SHALL BE PER LOCAL AGENCY STANDARDS FOR BUS STOPS. CURBS SHALL BE NON-MOUNTABLE TYPE.
2. WHERE CURB AND GUTTER IS NOT CONSTRUCTED INTEGRAL AS MONOLITHIC POUR, CONCRETE PAVEMENT AND GUTTER SHALL BE DOWELED PER CALTRANS STANDARD PLAN A-35B FOR LONGITUDINAL JOINTS
3. FOR DETAILS REGARDING JOINT SPACING, DOWELS, TIE BARS AND WEAKENED PLANE JOINTS, SEE LATEST APPROVED CALTRANS STANDARD PLANS.
4. JOINTS SHALL BE SEALED WITH POURED SEALANT AS APPROVED BY THE ENGINEER.
5. WEAKENED PLANE DOWELED TRANSVERSE JOINT SHALL BE CONSTRUCTED AT 12 FEET MAX SPACING.



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**FIGURE No. 8-3  
PAVEMENT AND CURB DETAIL  
FOR BUS TURNOUTS**

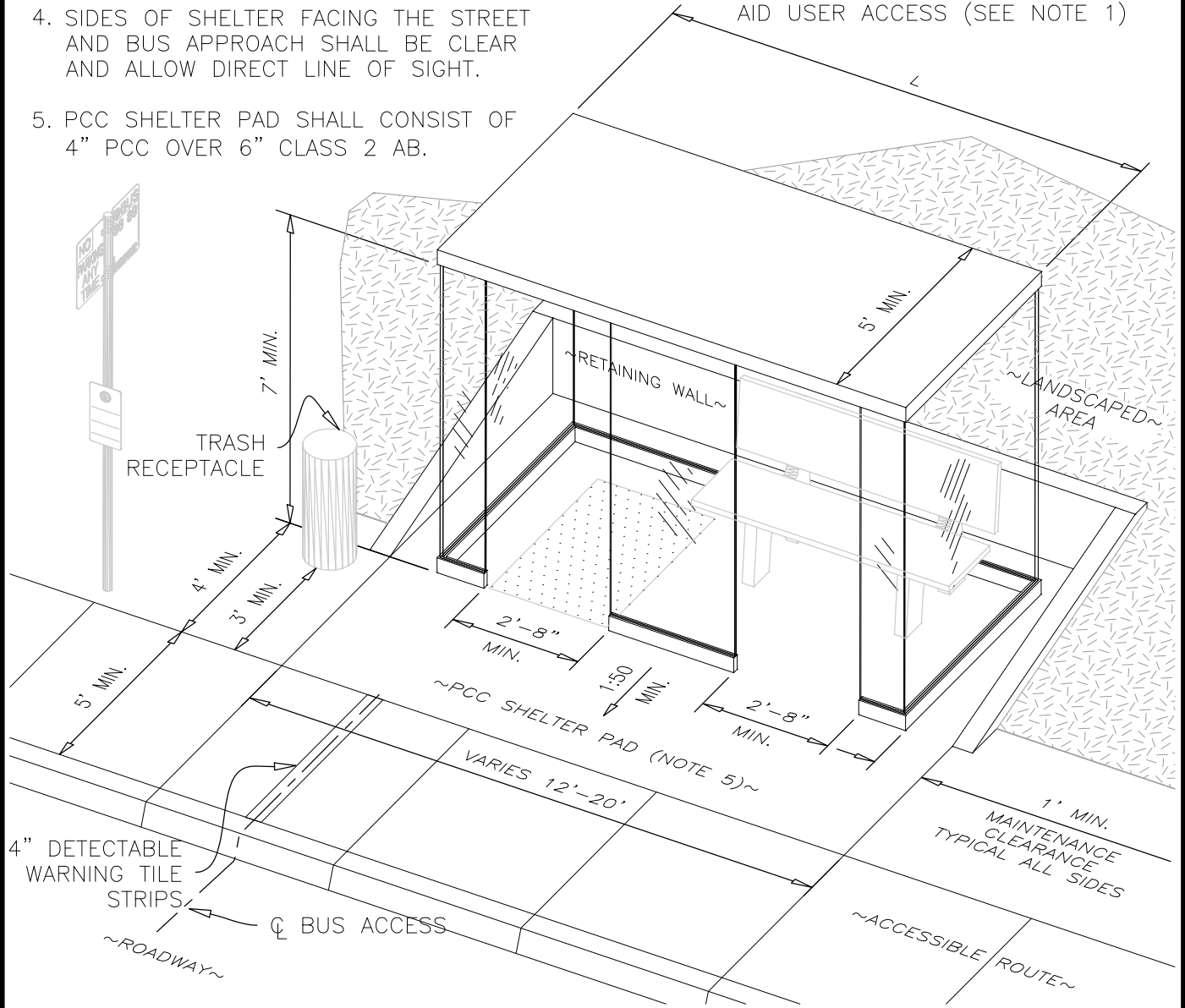
**NOTES:**

1. TWO OR MORE SPACES TO BE PROVIDED FOR 16' SHELTER OR AS WARRANTED BY SITE SPECIFIC CIRCUMSTANCES.
2. BUS STOP PAD MUST BE CLEAR OF UTILITY POLES, FIRE HYDRANTS, STREET FURNITURE OR SIMILAR OBSTACLES.
3. MINIMUM CLEARANCE FROM ANY PART OF BUS SHELTER INCLUDING ROOF SHALL BE 8'.
4. SIDES OF SHELTER FACING THE STREET AND BUS APPROACH SHALL BE CLEAR AND ALLOW DIRECT LINE OF SIGHT.
5. PCC SHELTER PAD SHALL CONSIST OF 4" PCC OVER 6" CLASS 2 AB.

L = MINIMUM LENGTH (ft.)

10'-0" LOW RIDERSHIP < 10 boarding/hr.
12'-0" MED RIDERSHIP 10-20 boarding/hr.
16'-0" HIGH RIDERSHIP > 20 boarding/hr.

 MINIMUM CLEAR FLOOR AREA (2'-6" WIDE BY 4' DEEP) ENTIRELY WITHIN PERIMETER OF SHELTER TO PERMIT WHEELCHAIR OR MOBILITY AID USER ACCESS (SEE NOTE 1)



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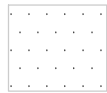
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**FIGURE No. 9-1**

**ACCESSIBLE BUS STOP PAD &  
SHELTER - SIDEWALK IN FRONT**



MINIMUM CLEAR FLOOR AREA (2'-6" WIDE BY 4' DEEP) ENTIRELY WITHIN PERIMETER OF SHELTER TO PERMIT WHEELCHAIR OR MOBILITY AID USER ACCESS (SEE NOTE 1)



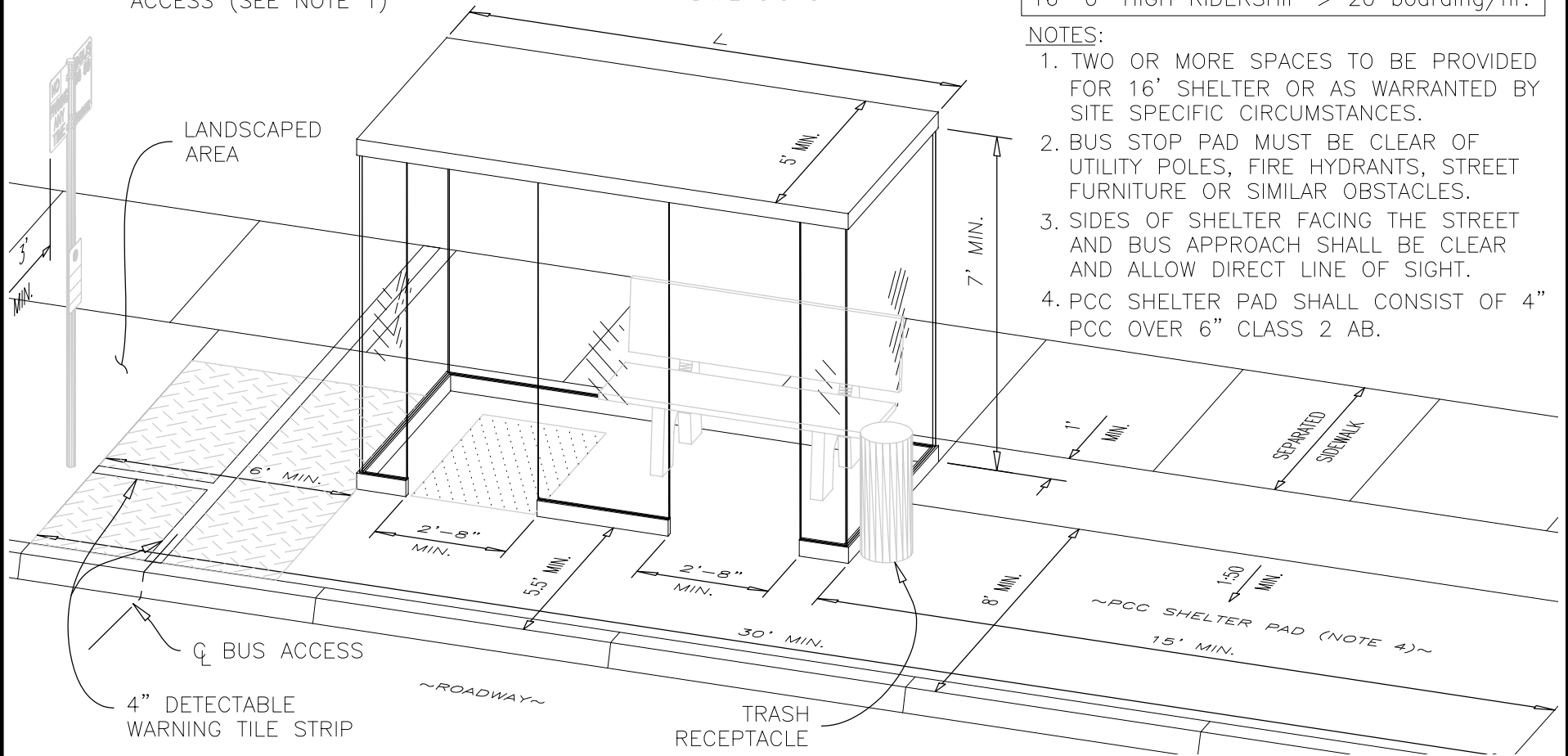
A CLEAR AREA SHALL BE AVAILABLE FOR WHEELCHAIR LOADING AREA, REFER TO FIGURE NO. 7-6 FOR DIMENSIONS.

L = MINIMUM LENGTH (ft.)

10'-0" LOW RIDERSHIP < 10 boarding/hr.
12'-0" MED RIDERSHIP 10-20 boarding/hr.
16'-0" HIGH RIDERSHIP > 20 boarding/hr.

NOTES:

1. TWO OR MORE SPACES TO BE PROVIDED FOR 16' SHELTER OR AS WARRANTED BY SITE SPECIFIC CIRCUMSTANCES.
2. BUS STOP PAD MUST BE CLEAR OF UTILITY POLES, FIRE HYDRANTS, STREET FURNITURE OR SIMILAR OBSTACLES.
3. SIDES OF SHELTER FACING THE STREET AND BUS APPROACH SHALL BE CLEAR AND ALLOW DIRECT LINE OF SIGHT.
4. PCC SHELTER PAD SHALL CONSIST OF 4" PCC OVER 6" CLASS 2 AB.



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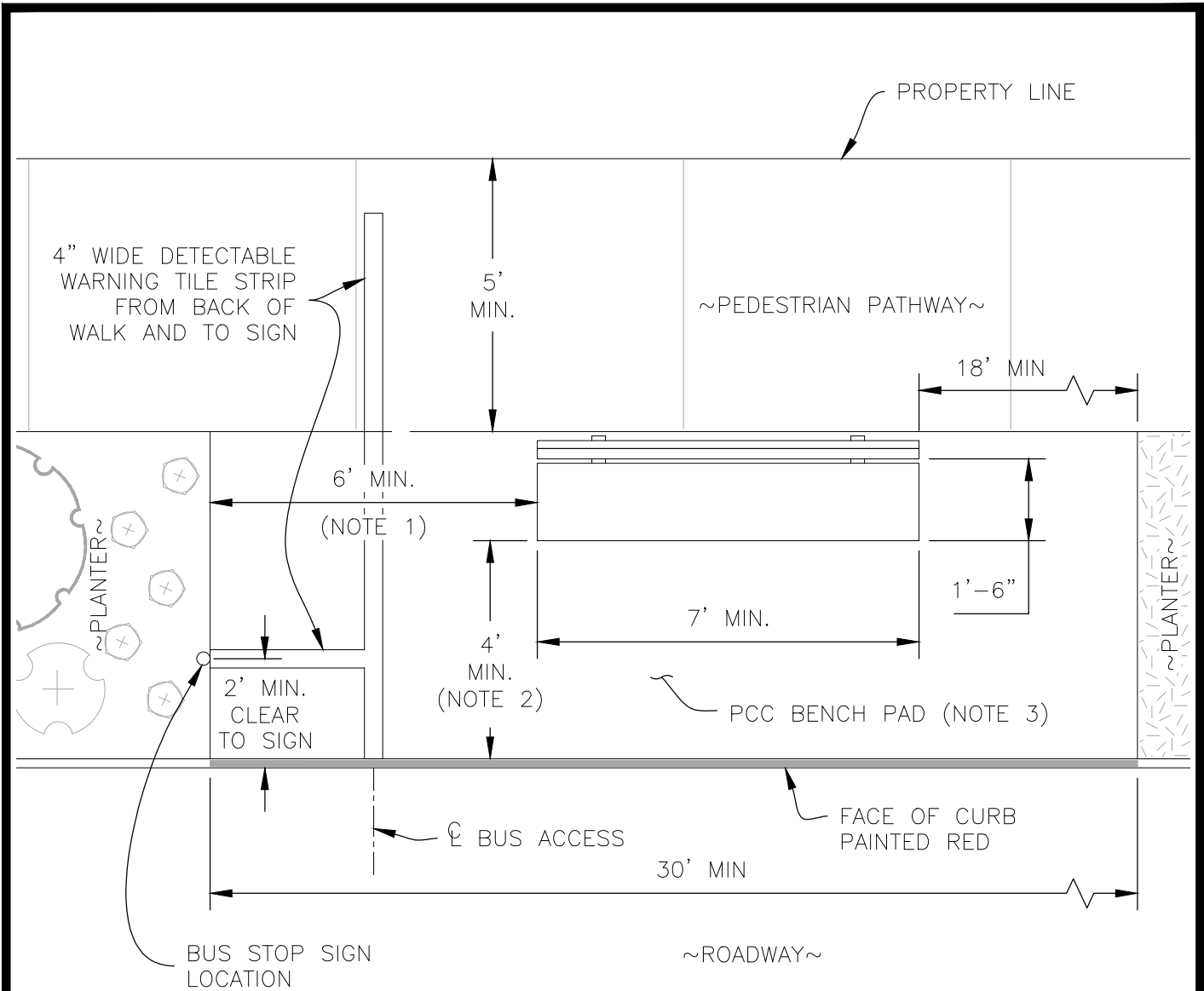
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FIGURE No. 9-2  
ACCESSIBLE BUS STOP  
PAD & SHELTER





NOTE:

1. A CLEAR AREA SHALL BE AVAILABLE FOR WHEELCHAIR LOADING AREA, REFER TO FIGURE NO. 7-6 FOR DIMENSIONS.
2. PREFERRED DISTANCE OF 5.0'
3. PCC BENCH PAD SHALL CONSIST OF 4" PCC OVER 6" CLASS 2 AB.



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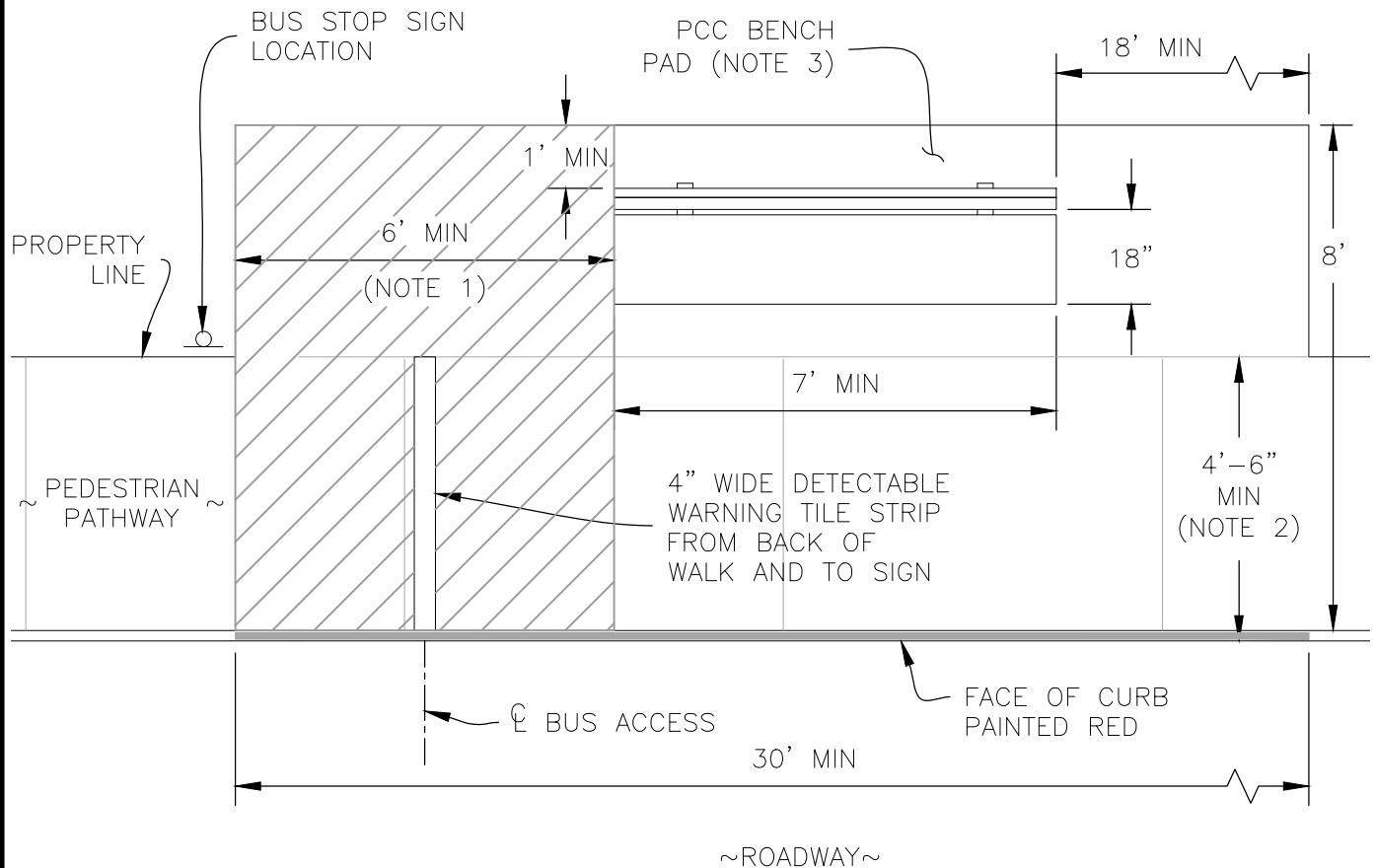
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**FIGURE No.10-1  
BUS BENCH PLACEMENT  
(SEPARATED SIDEWALK)**



NOTE:

1. A CLEAR AREA SHALL BE AVAILABLE FOR WHEELCHAIR LOADING AREA, REFER TO FIGURE NO. 7-6 FOR DIMENSIONS.
2. PREFERRED DISTANCE OF 6.0'
3. PCC BENCH PAD SHALL CONSIST OF 4" PCC OVER 6" CLASS 2 AB.

LEGEND:



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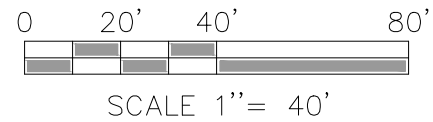
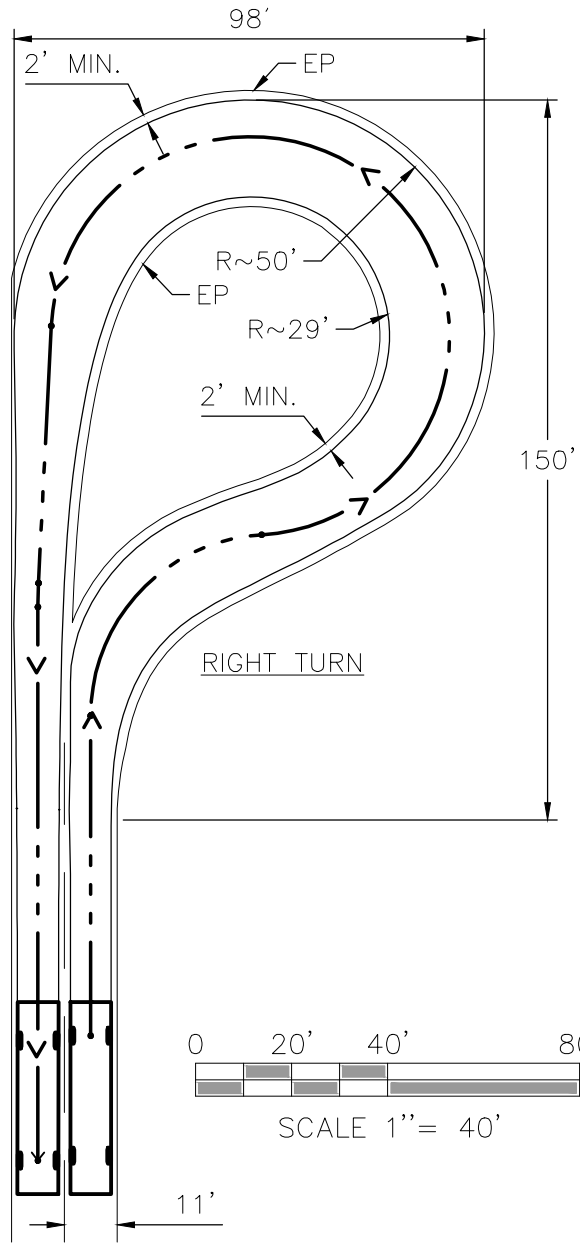
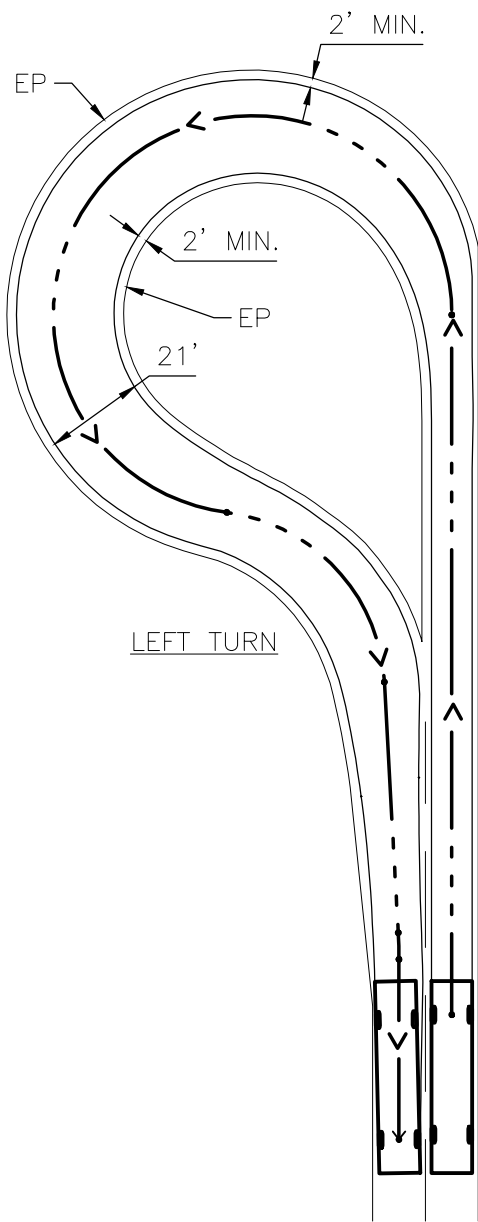
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**FIGURE No.10-2**

**BUS BENCH PLACEMENT  
(SIDEWALK ADJACENT TO CURB)**



**NOTES:**

1. ALL CURBS IN TURNAROUND SHALL BE NON-MOUNTABLE CURBS AND SHALL BE PAINTED RED.
2. PAVEMENT FOR BUS TURNAROUNDS SHALL BE CONCRETE PAVEMENT PER THE REQUIREMENTS OF SECTION 4 FOR DOWEL JOINTED CONCRETE PAVEMENTS.
3. R26 AND R26(S) SIGNS SHALL BE PLACED AT 50 FT INTERVALS AROUND THE PERIMETER OF THE TURNAROUND.



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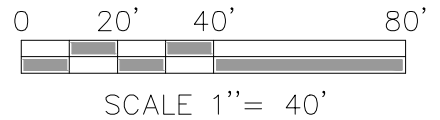
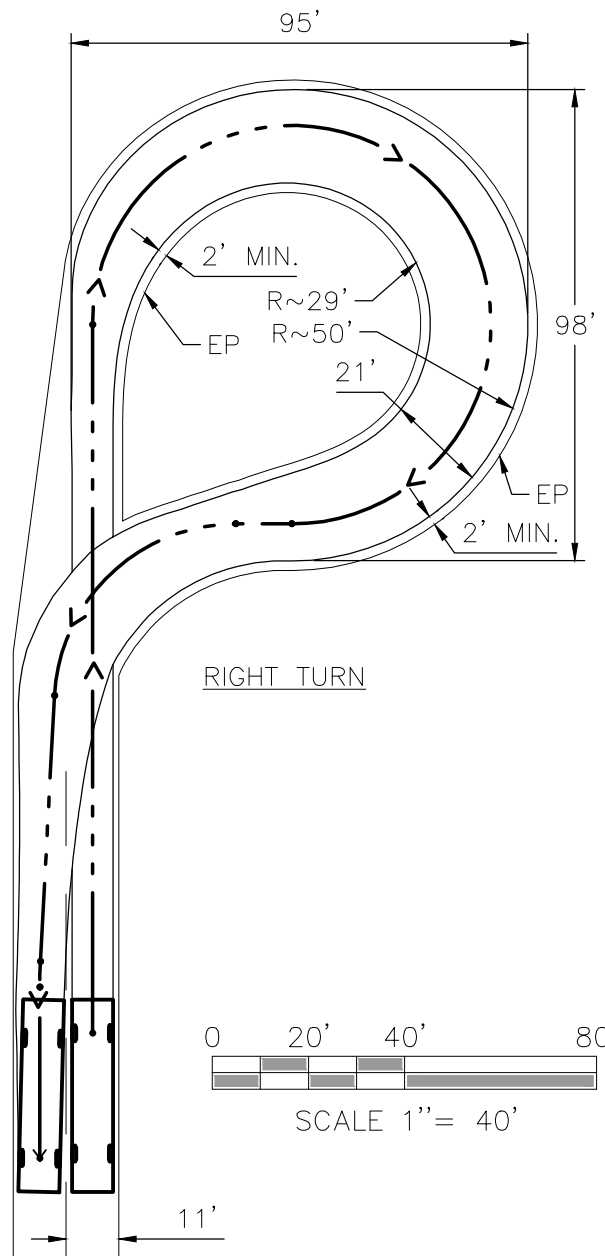
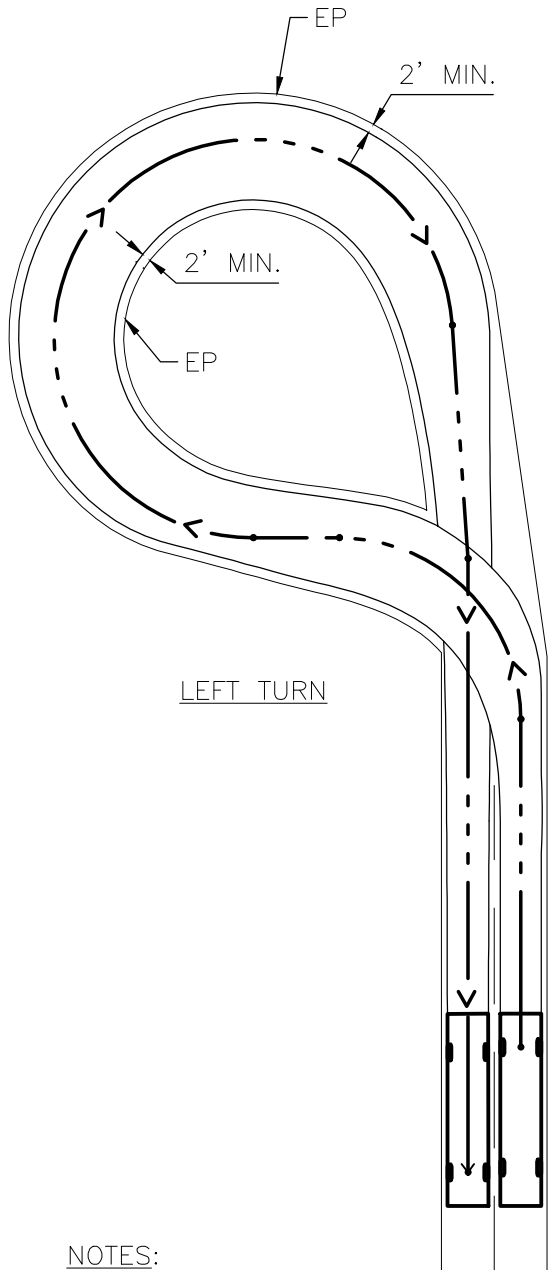
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**FIGURE No.12-1  
COUNTER-CLOCKWISE  
TURNAROUND**



**NOTES:**

1. ALL CURBS IN TURNAROUND SHALL BE NON-MOUNTABLE CURBS AND SHALL BE PAINTED RED.
2. PAVEMENT FOR BUS TURNAROUNDS SHALL BE CONCRETE PAVEMENT PER THE REQUIREMENTS OF SECTION 3-4 FOR DOWEL JOINTED CONCRETE PAVEMENTS.
3. R26 AND R26(S) SIGNS SHALL BE PLACED AT 50 FT INTERVALS AROUND THE PERIMETER OF THE TURNAROUND.



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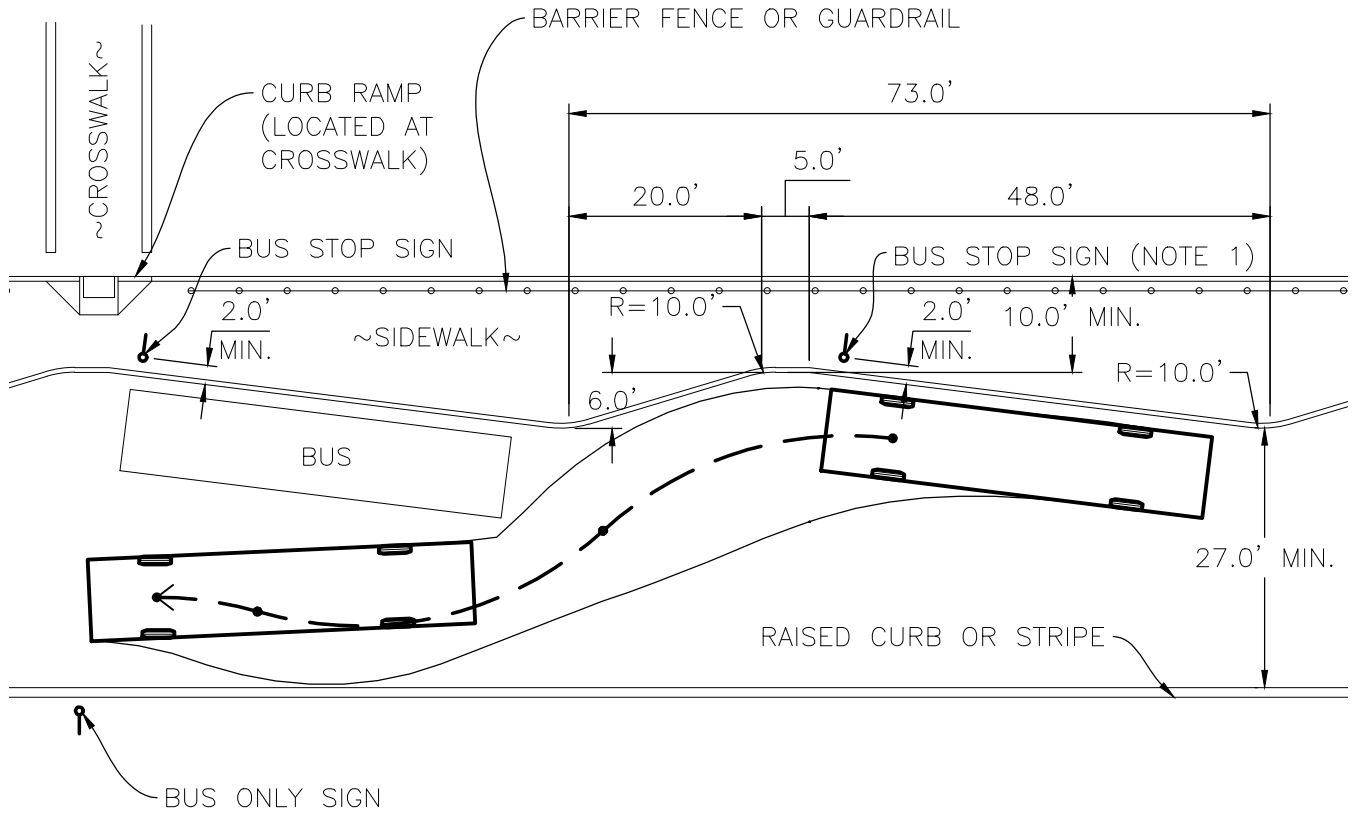
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**FIGURE No. 12-2  
CLOCKWISE TURNAROUND**



NOTE:

1. FOR SIGN LOCATION REFER TO SECTION 3, BUS STOPS.



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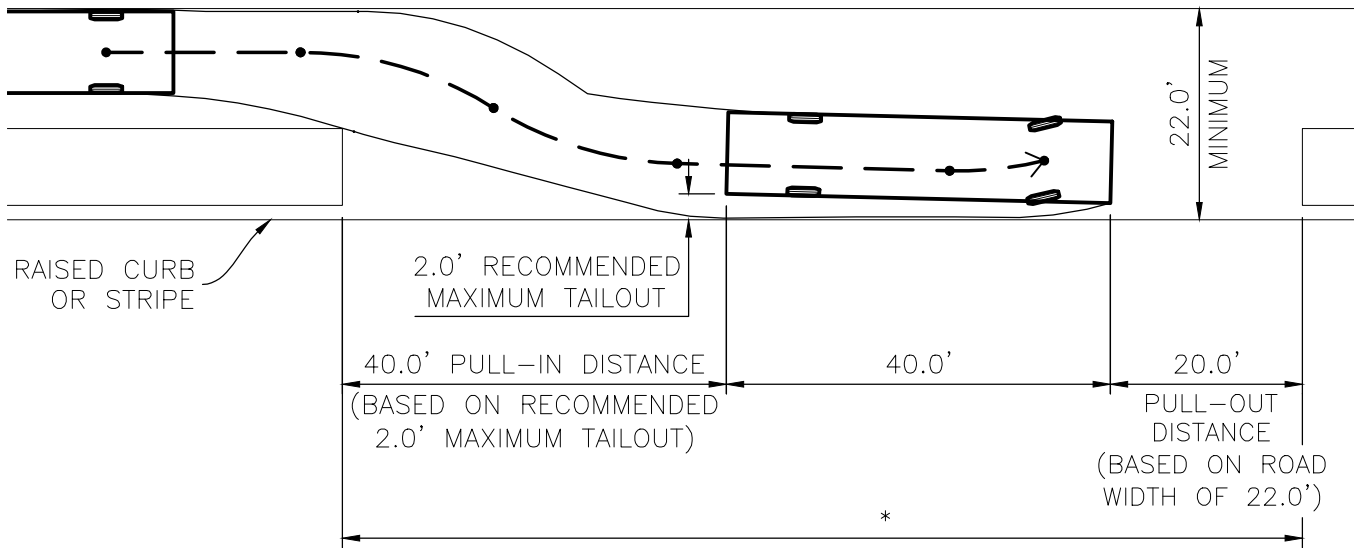
APPROVED:

*[Signature]*  
DIRECTOR, ENGINEERING AND CONSTRUCTION

DATE: \_\_\_\_\_

FIGURE No. 14-1  
SAWTOOTH BERTH

RECOMMENDED BERTH

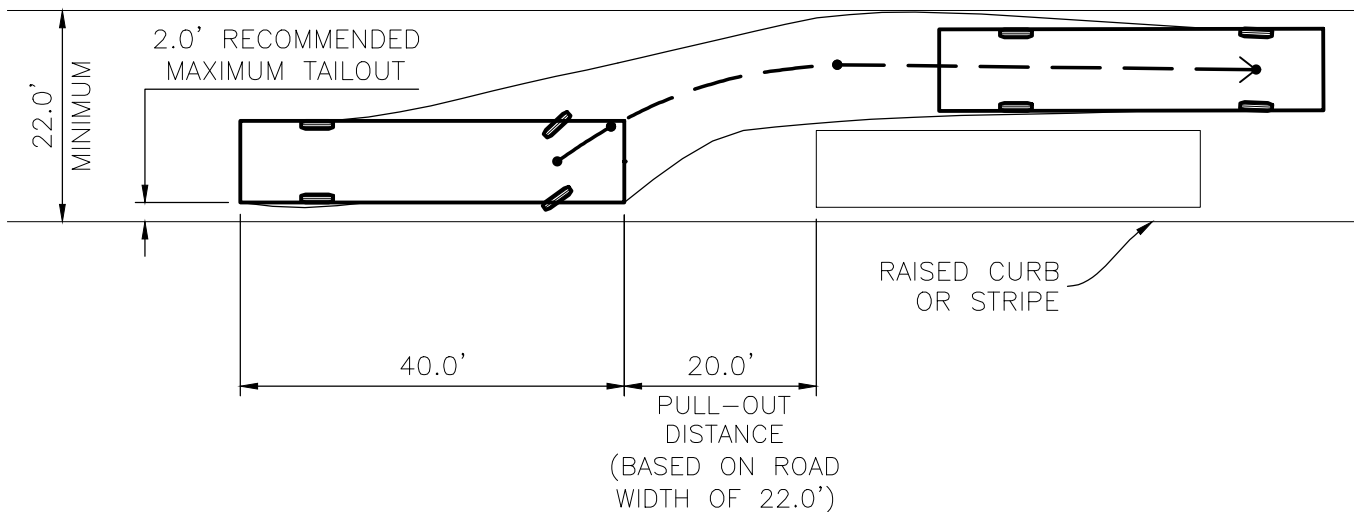


\* LENGTH OF PARALLEL BERTH (L) = PULL-IN DISTANCE + LENGTH OF BUS + PULL-OUT DISTANCE. LENGTH WILL VARY DEPENDING ON ALLOWABLE TAILOUT AND ROAD WIDTH. TO DETERMINE PROPER LENGTH, USE TABLE 8-1.

NOTE:

1. REFER TO SECTION 3, BUS STOPS FOR SIGN LOCATIONS.

PULL-OUT CONFIGURATION



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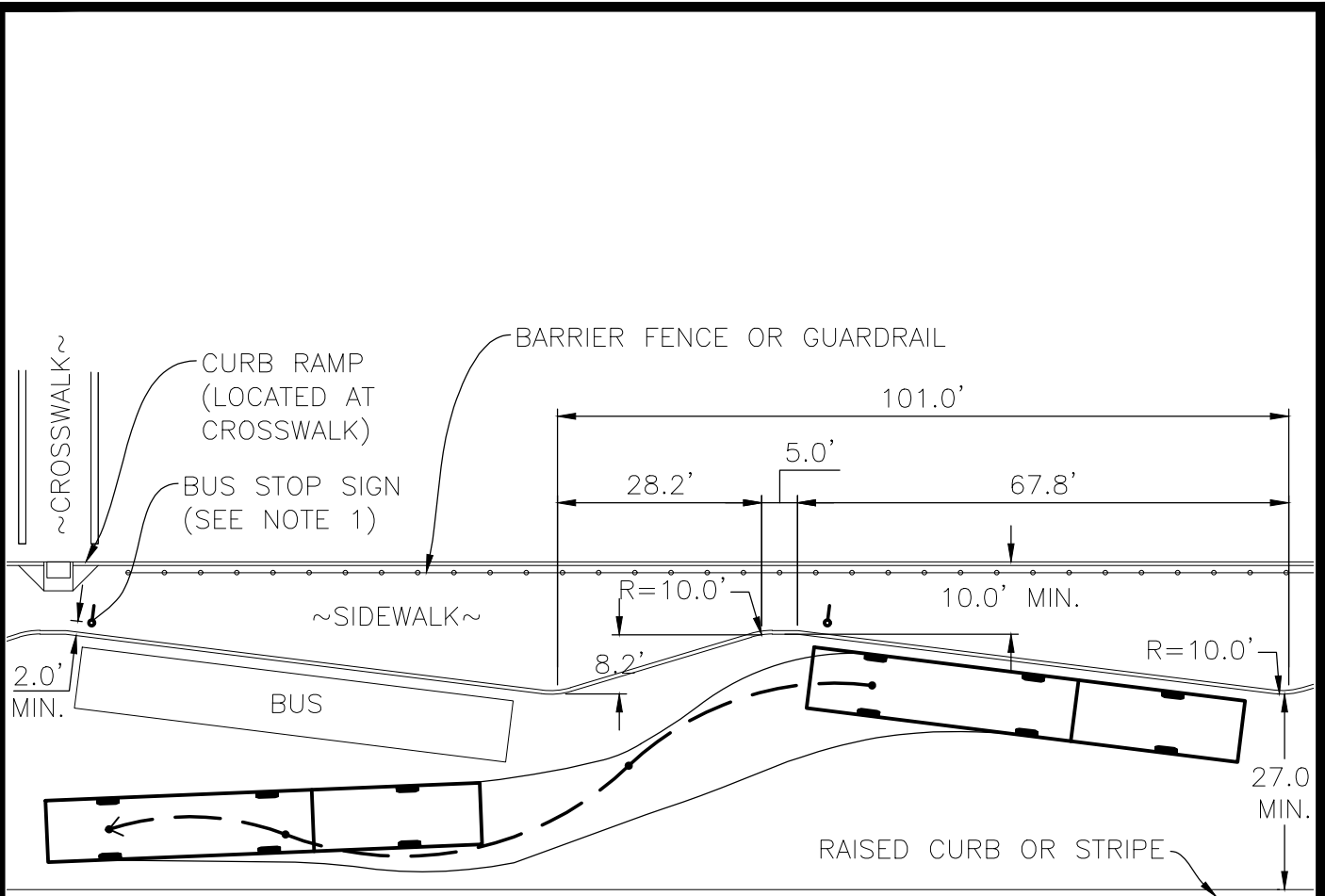
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FIGURE No. 14-2  
PARALLEL BERTH



BUS ONLY SIGN

NOTE:  
 1. FOR SIGN LOCATION REFER TO SECTION 3, BUS STOPS.



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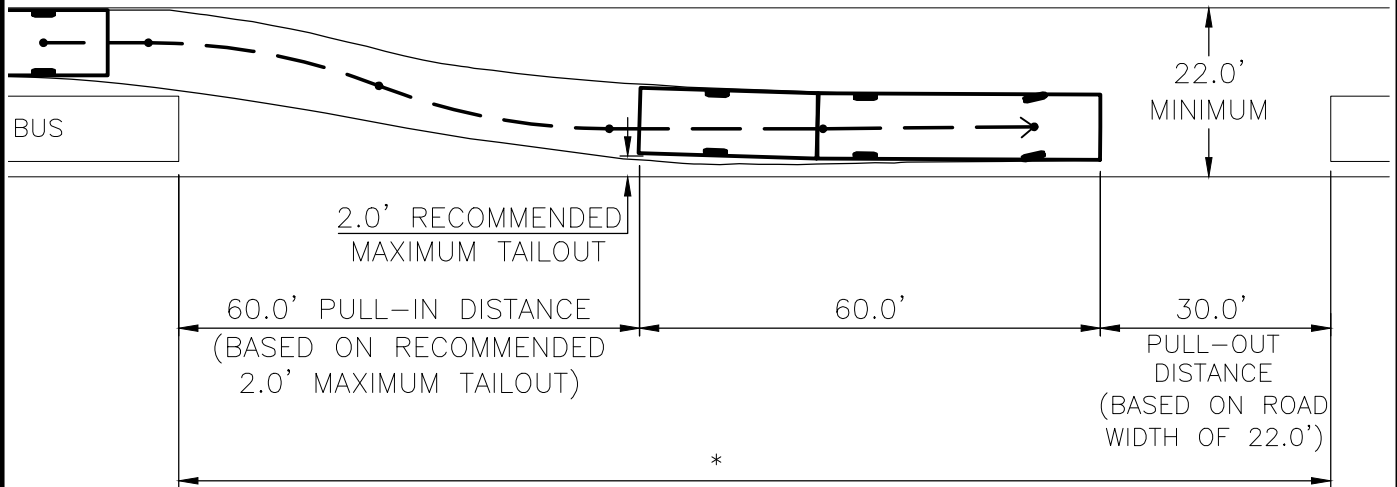
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FIGURE No. 14-3  
 SAWTOOTH BERTH  
 ARTICULATED 60' BUS

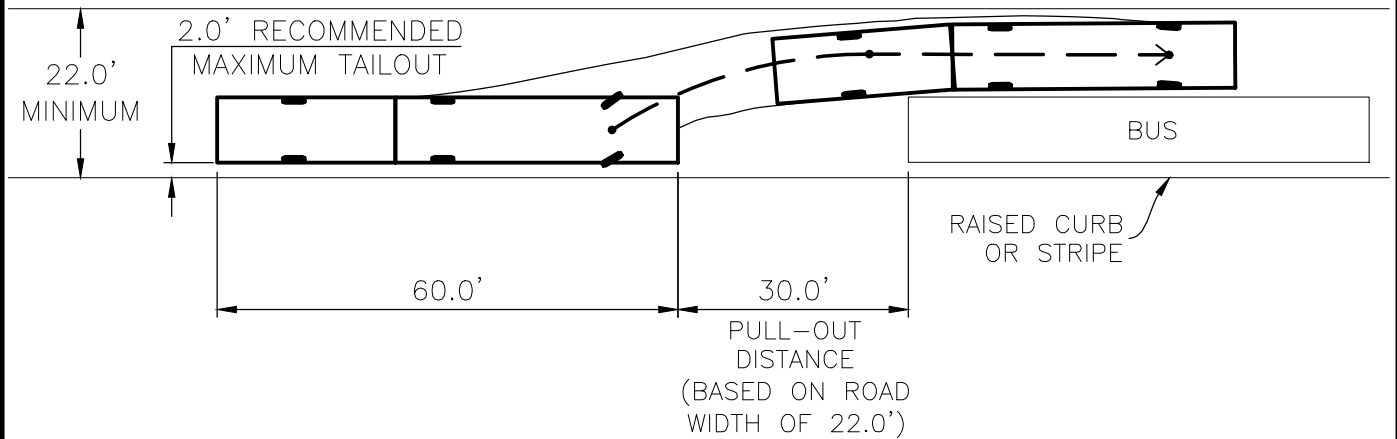
RECOMMENDED BERTH



NOTE:

1. REFER TO SECTION 3, BUS STOPS FOR SIGN LOCATIONS.

PULL-OUT CONFIGURATION



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**DESIGN GUIDELINES**

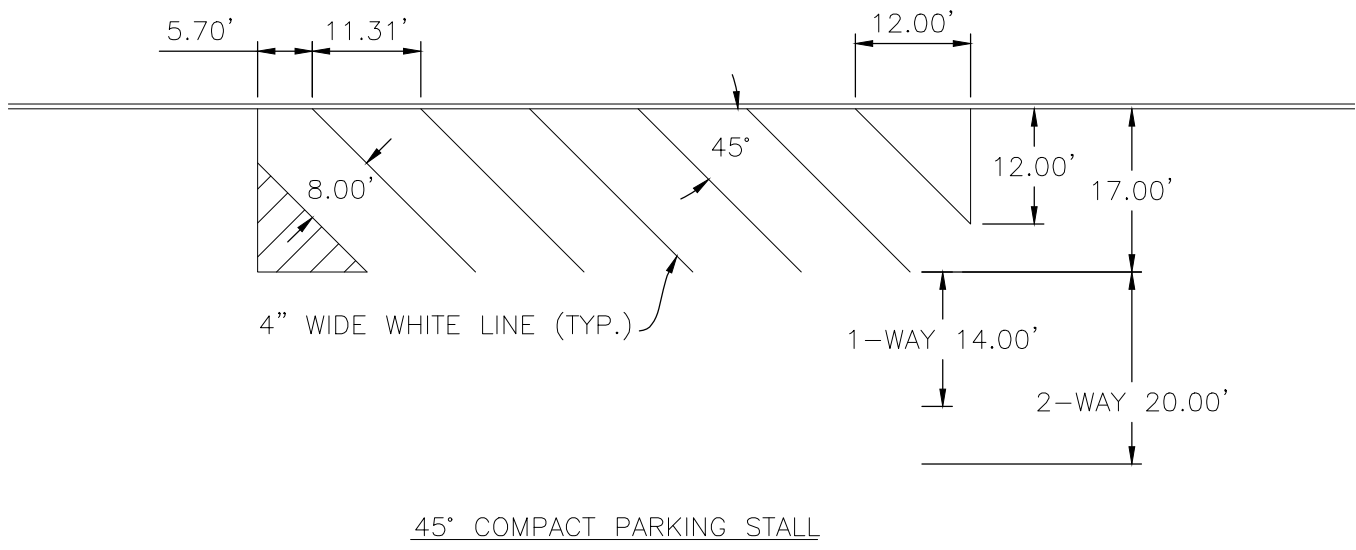
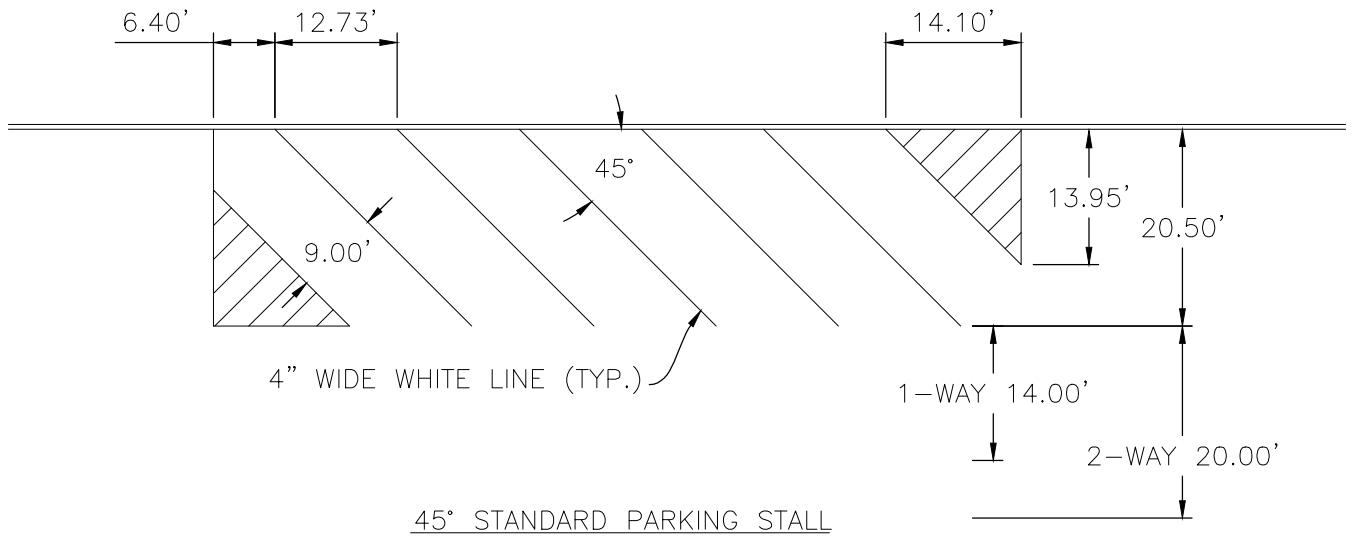
APPROVED:

*[Signature]*  
\_\_\_\_\_  
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**FIGURE No. 14-4  
PARALLEL BERTH  
ARTICULATED 60' BUS**



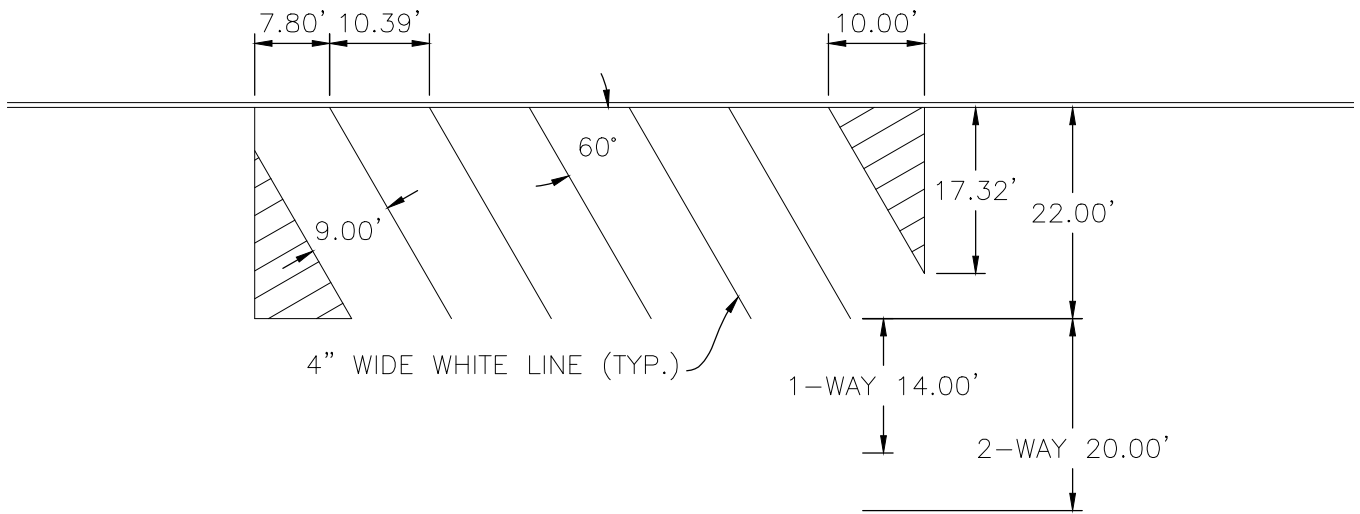


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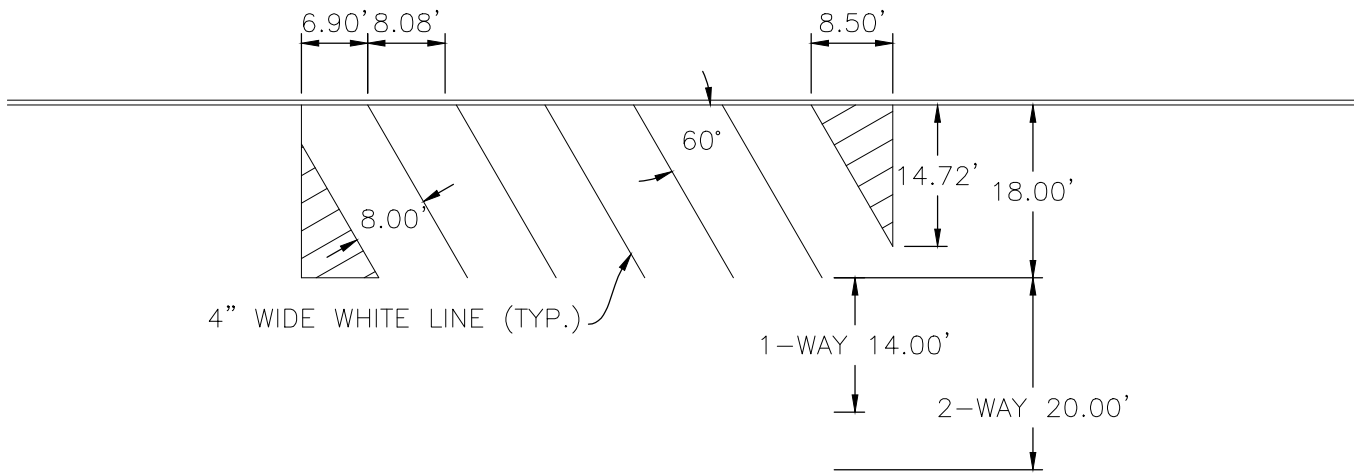
**DESIGN GUIDELINES**

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**FIGURE No. 15-1  
 45° PARKING STALL LAYOUT**



60° STANDARD PARKING STALL



60° COMPACT PARKING STALL



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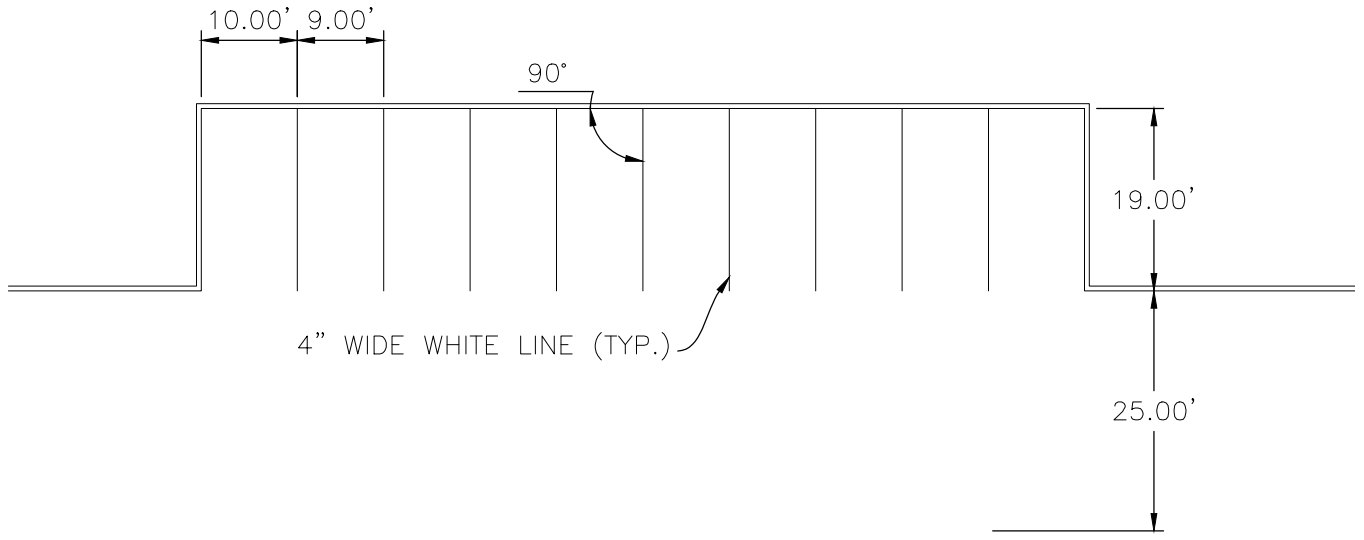
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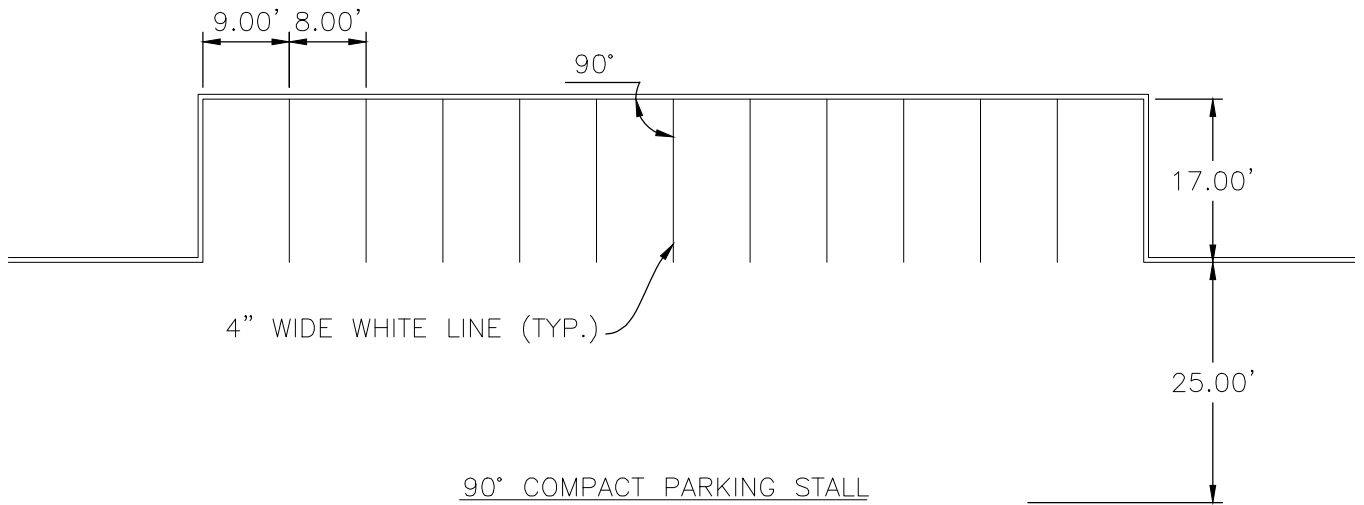
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**FIGURE No. 15-2**  
**60° PARKING STALL LAYOUT**



90° STANDARD PARKING STALL



90° COMPACT PARKING STALL



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**DESIGN GUIDELINES**

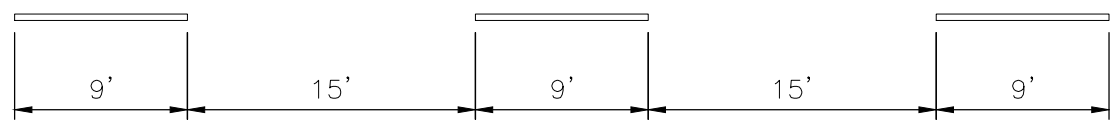
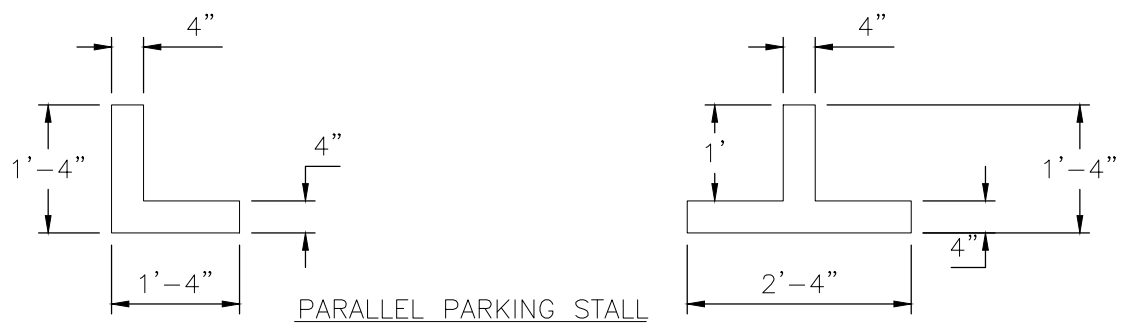
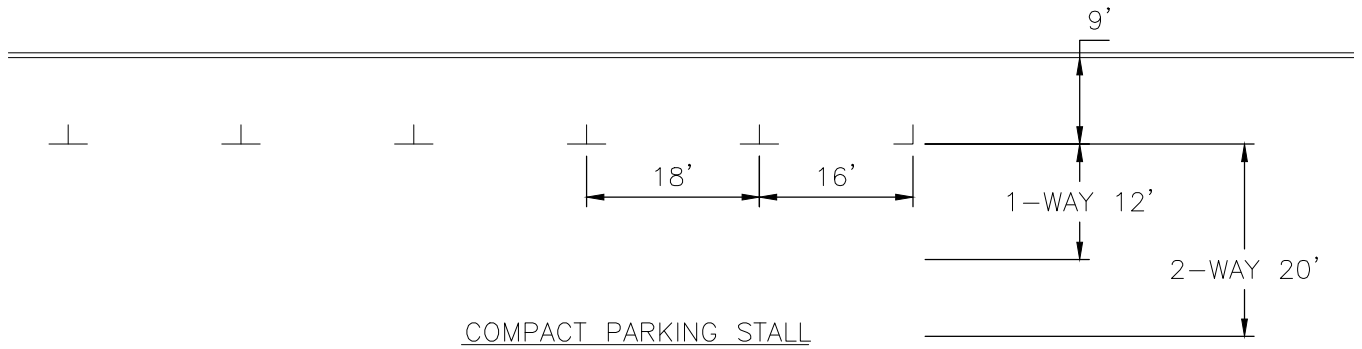
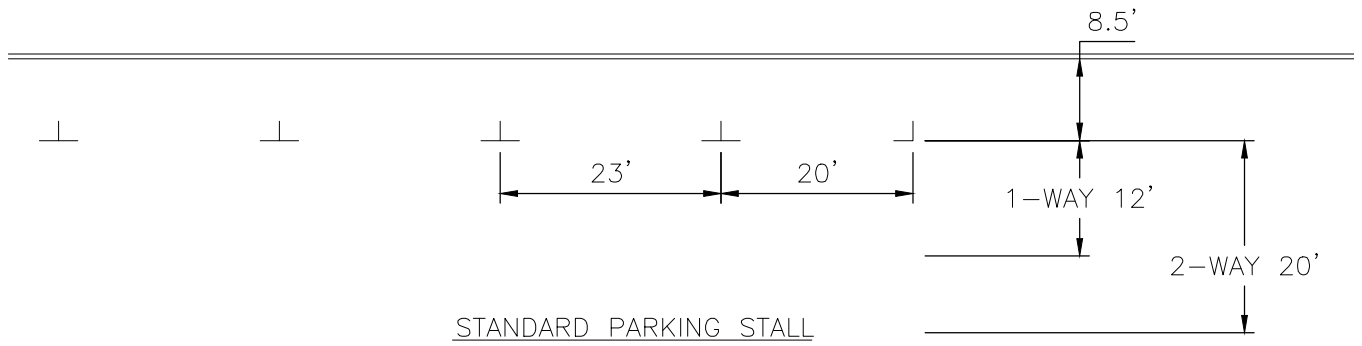
APPROVED:

*[Handwritten Signature]*

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**FIGURE No. 15-3  
90° PARKING STALL LAYOUT**



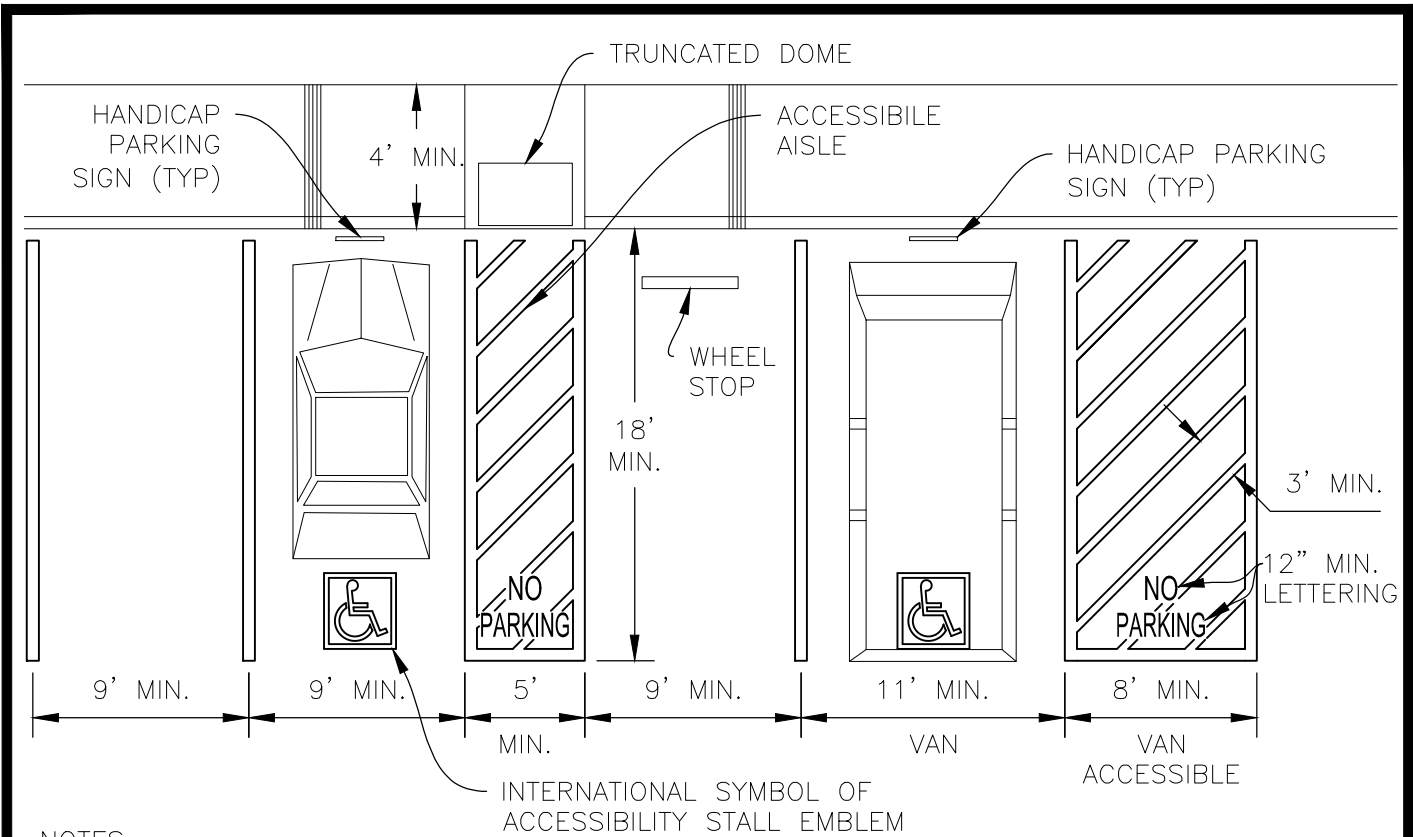
4" WIDE STRIP— A.) WHITE PAINT FOR ONE WAY TRAFFIC  
 B.) YELLOW PAINT FOR TWO WAY TRAFFIC



# DESIGN GUIDELINES

APPROVED: *[Signature]*  
 DIRECTOR, ENGINEERING AND CONSTRUCTION  
 DATE: \_\_\_\_\_

FIGURE No. 15-4  
**PARALLEL PARKING STALL LAYOUT**



**NOTES:**

1. DISABLED PARKING SPACE MUST PERMIT USE OF EITHER CAR DOORS.
2. A PARKING BUMPER (WHEEL STOP) IS REQUIRED WHEN NO CURB OR BARRIER IS PROVIDED WHICH WILL PREVENT ENCROACHMENT OF CARS OVER THE ADJOINING ACCESSIBLE ROUTE.
3. WHEELCHAIR USERS MUST NOT BE FORCED TO GO BEHIND PARKED CARS OTHER THAN THEIR OWN TO ACCESS AN ADJOINING ACCESSIBLE ROUTE.
4. THE MAXIMUM SURFACE SLOPE WITHIN THE DISABLED PARKING SPACE AND ADJACENT ACCESS AISLE MAY NOT EXCEED 2% IN ANY DIRECTION.
5. CURB RAMPS MAY NOT ENCROACH INTO THE REQUIRED DIMENSIONS OF DISABLED PARKING SPACES OR ADJACENT ACCESS AISLES.
6. ACCESS AISLE (LOADING & UNLOADING AREA) MUST CONNECT TO AN ACCESSIBLE PATH OF TRAVEL TO THE FACILITY.
7. THE WORDS "NO PARKING" MUST BE PAINTED ON THE GROUND WITHIN EACH ACCESS AISLE.
8. THE DESIGNER SHALL CONFIRM THE LATEST ADA AND TITLE 24 REQUIREMENTS REGARDING THE NUMBER AND LAYOUT OF DISABLED PARKING STALLS PRIOR TO USING THE STANDARDS SPECIFIED IN THESE GUIDELINES.



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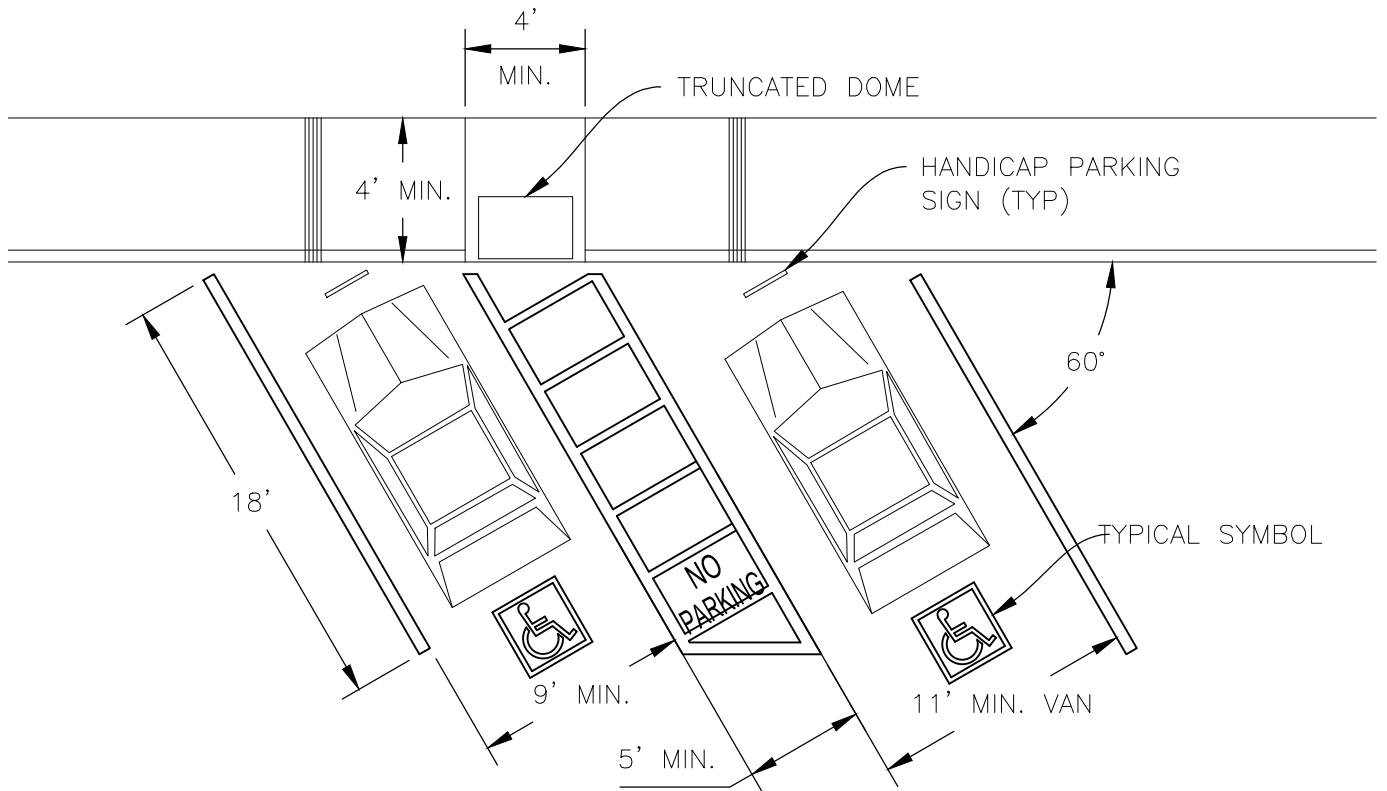
**DESIGN GUIDELINES**

APPROVED:

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DATE:

**FIGURE No. 15-5  
DISABLED PERPENDICULAR  
PARKING LAYOUT**



**NOTES:**

1. DISABLED PARKING SPACE MUST PERMIT USE OF EITHER CAR DOORS.
2. A PARKING BUMPER IS REQUIRED WHEN NO CURB OR BARRIER IS PROVIDED WHICH WILL PREVENT ENCROACHMENT OF CARS OVER THE ADJOINING ACCESSIBLE ROUTE.
3. WHEELCHAIR USERS MUST NOT BE FORCED TO GO BEHIND PARKED CARS OTHER THAN THEIR OWN TO ACCESS AN ADJOINING ACCESSIBLE ROUTE.
4. THE MAXIMUM SURFACE SLOPE WITHIN THE DISABLED PARKING SPACE AND ADJACENT ACCESS AISLE MAY NOT EXCEED 2% IN ANY DIRECTION.
5. CURB RAMPS MAY NOT ENCROACH INTO THE REQUIRED DIMENSIONS OF DISABLED PARKING SPACES OR ADJACENT ACCESS AISLES.
6. ACCESS AISLE (LOADING & UNLOADING AREA) MUST CONNECT TO AN ACCESSIBLE PATH OF TRAVEL TO THE FACILITY.
7. THE WORDS "NO PARKING" MUST BE PAINTED ON THE GROUND WITHIN EACH ACCESS AISLE.
8. THE DESIGNER SHALL CONFIRM THE LATEST ADA AND TITLE 24 REQUIREMENTS REGARDING THE NUMBER AND LAYOUT OFF DISABLED PARKING STALLS PRIOR TO USING THE STANDARDS SPECIFIED IN THESE GUIDELINES.



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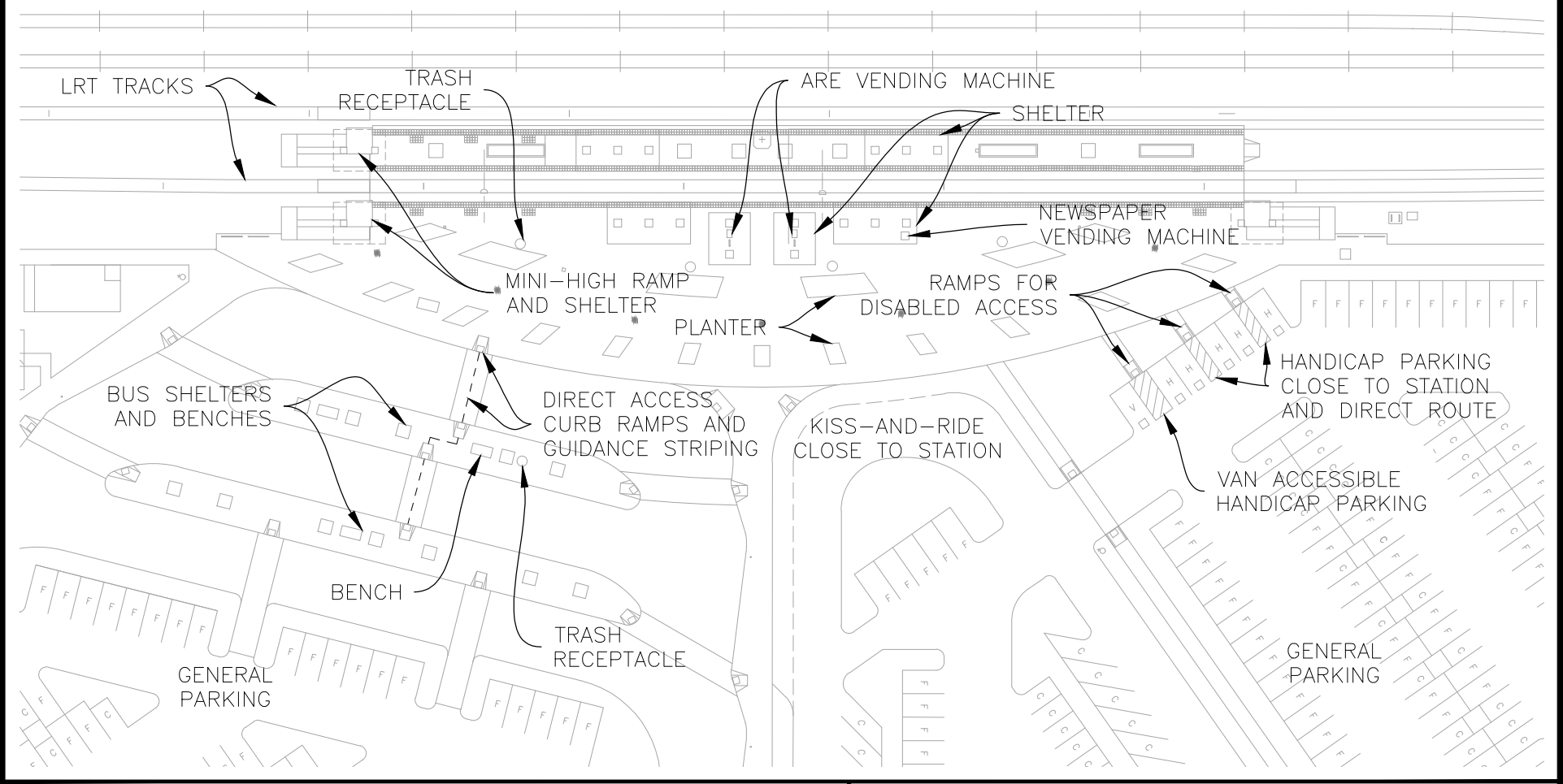
APPROVED:

*[Signature]*

DIRECTOR, ENGINEERING AND CONSTRUCTION

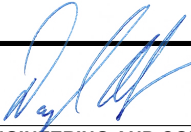
DATE: \_\_\_\_\_

**FIGURE No. 15-6  
DISABLED DIAGONAL  
PARKING LAYOUT**

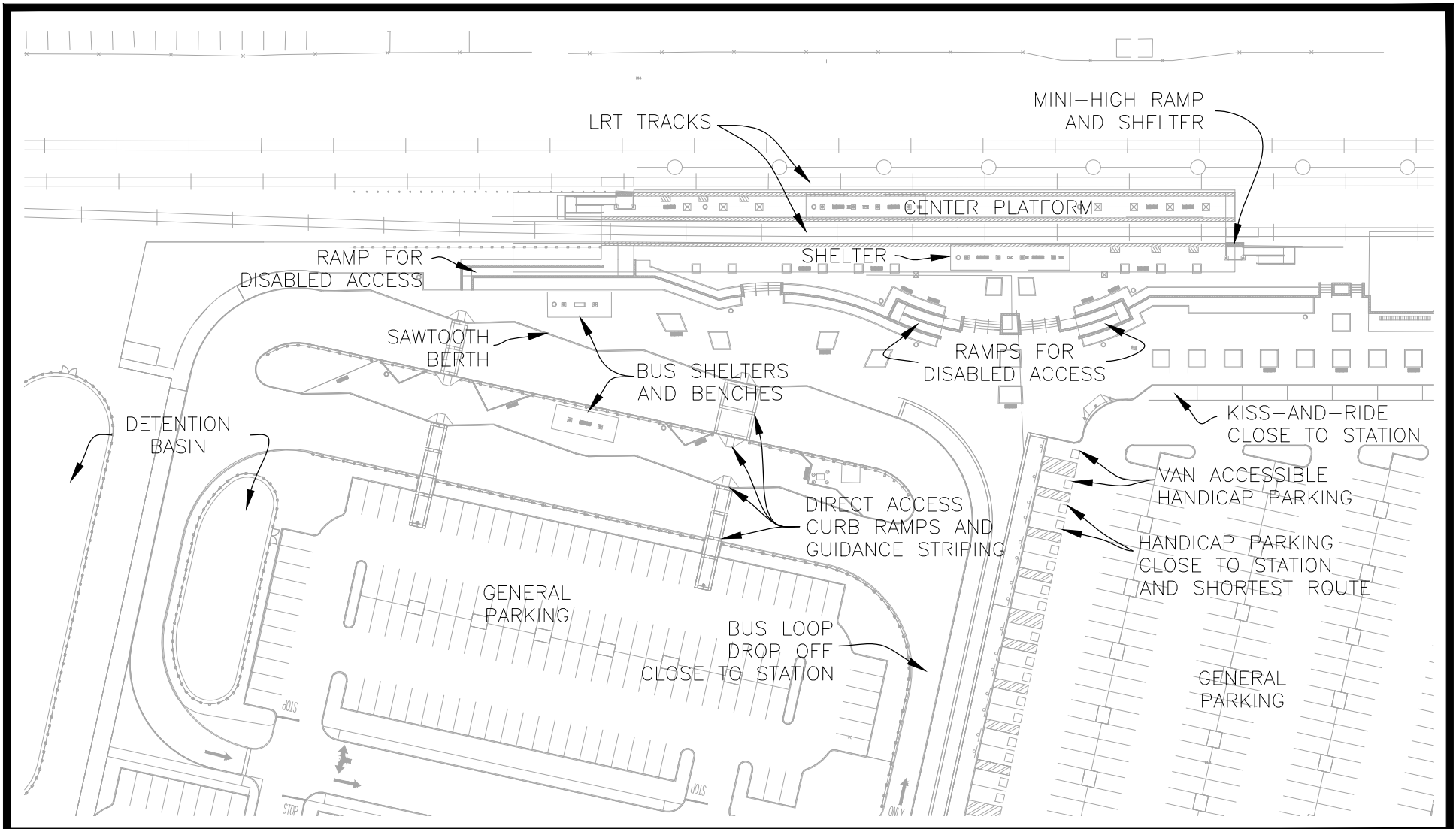


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**FIGURE No. 16-1(a)**  
**TYPICAL STATION LAYOUT**  
**AT LIGHT RAIL PARK-AND-RIDE FACILITY**



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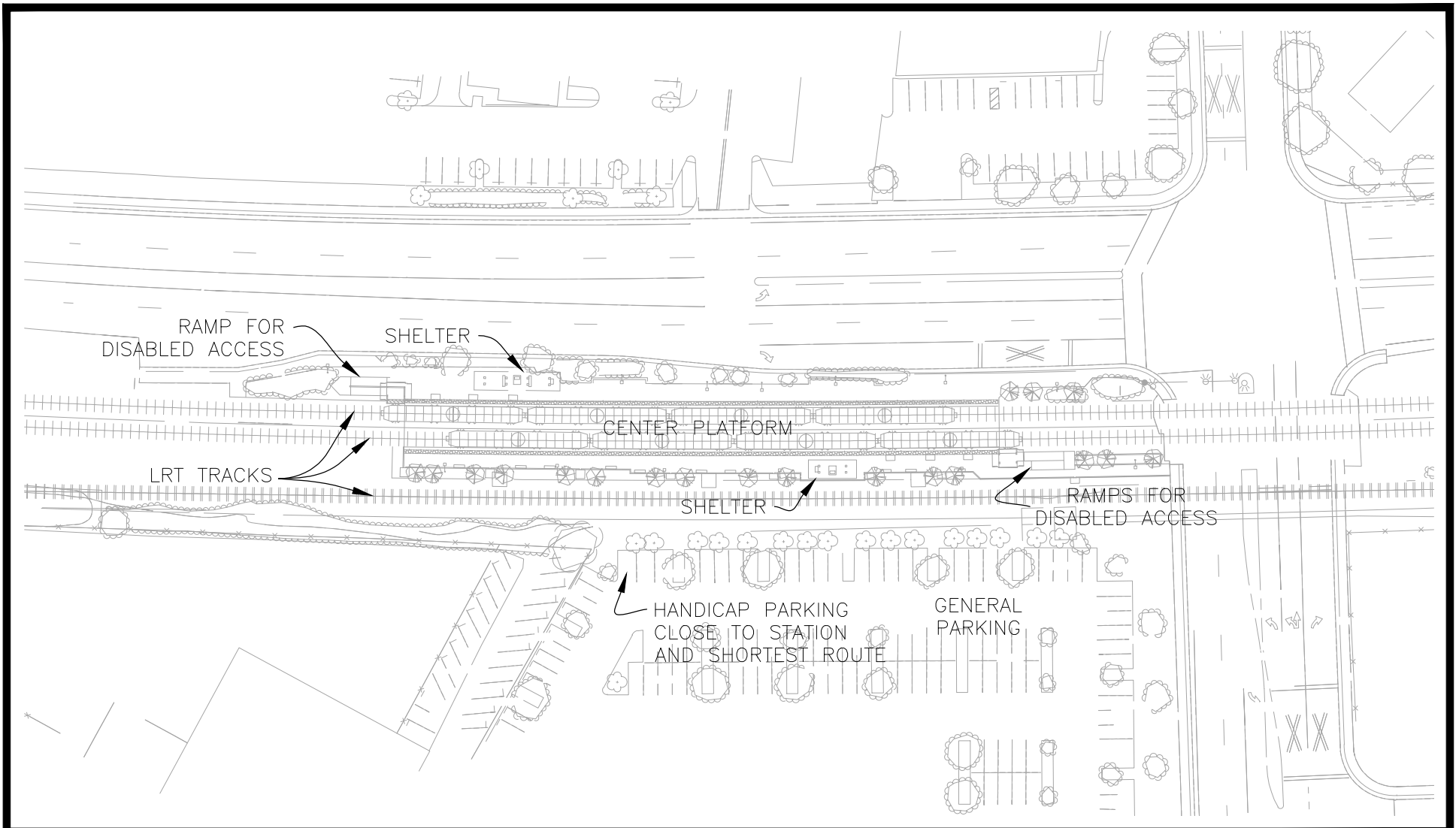
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FIGURE No. 16-1(b)

**TYPICAL STATION LAYOUT  
AT LIGHT RAIL PARK-AND-RIDE FACILITY**





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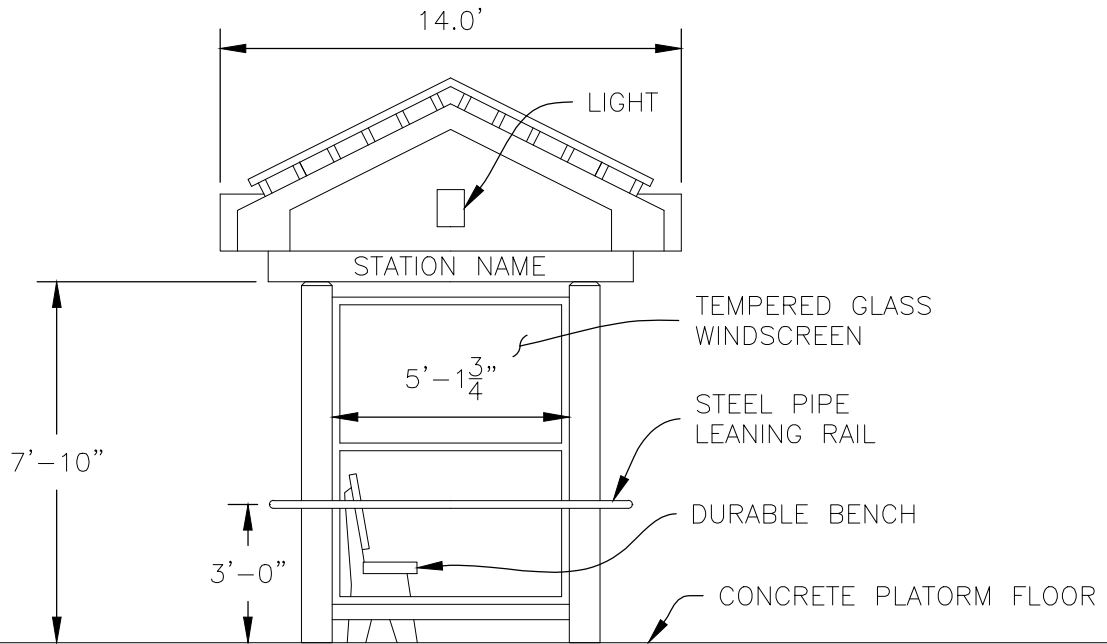
APPROVED:

DIRECTOR, ENGINEERING AND CONSTRUCTION

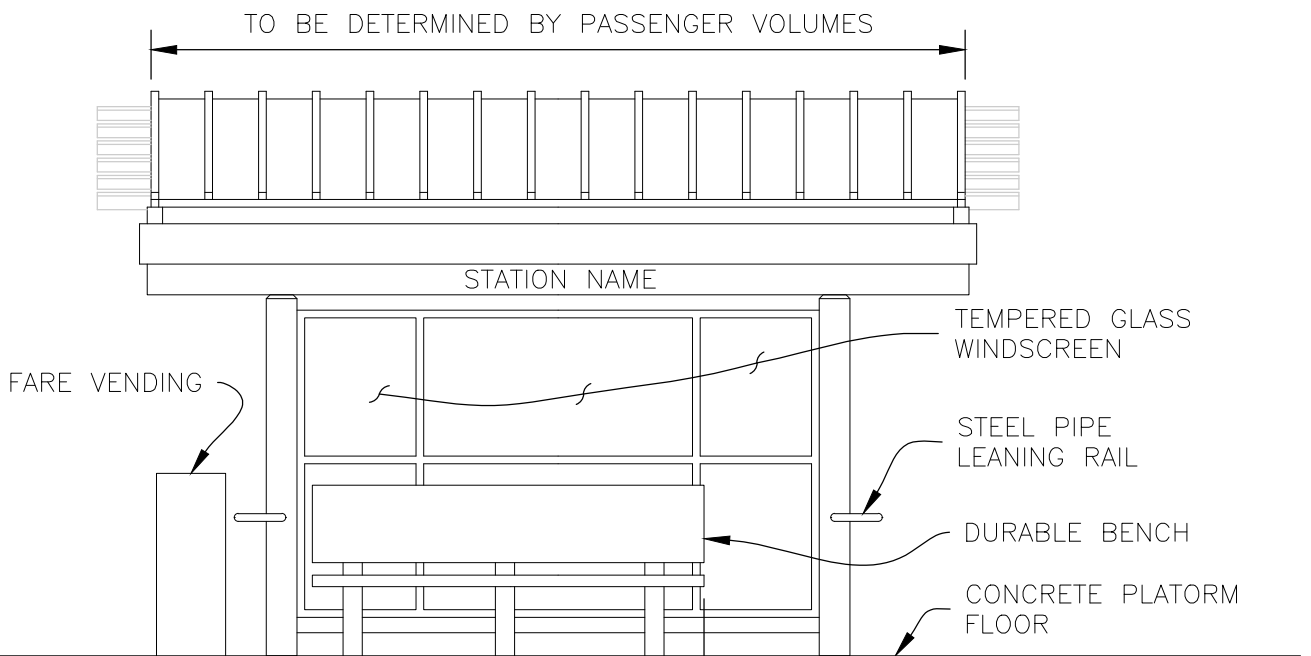
DATE:

FIGURE No. 16-1(c)

**TYPICAL STATION LAYOUT  
AT LIGHT RAIL PARK-AND-RIDE FACILITY**



END ELEVATION



FRONT ELEVATION

\* A MINIMUM CLEAR SPACE OF 2.5' WIDE BY 4.0' DEEP SHALL BE PROVIDED AT EACH SHELTER.



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## DESIGN GUIDELINES

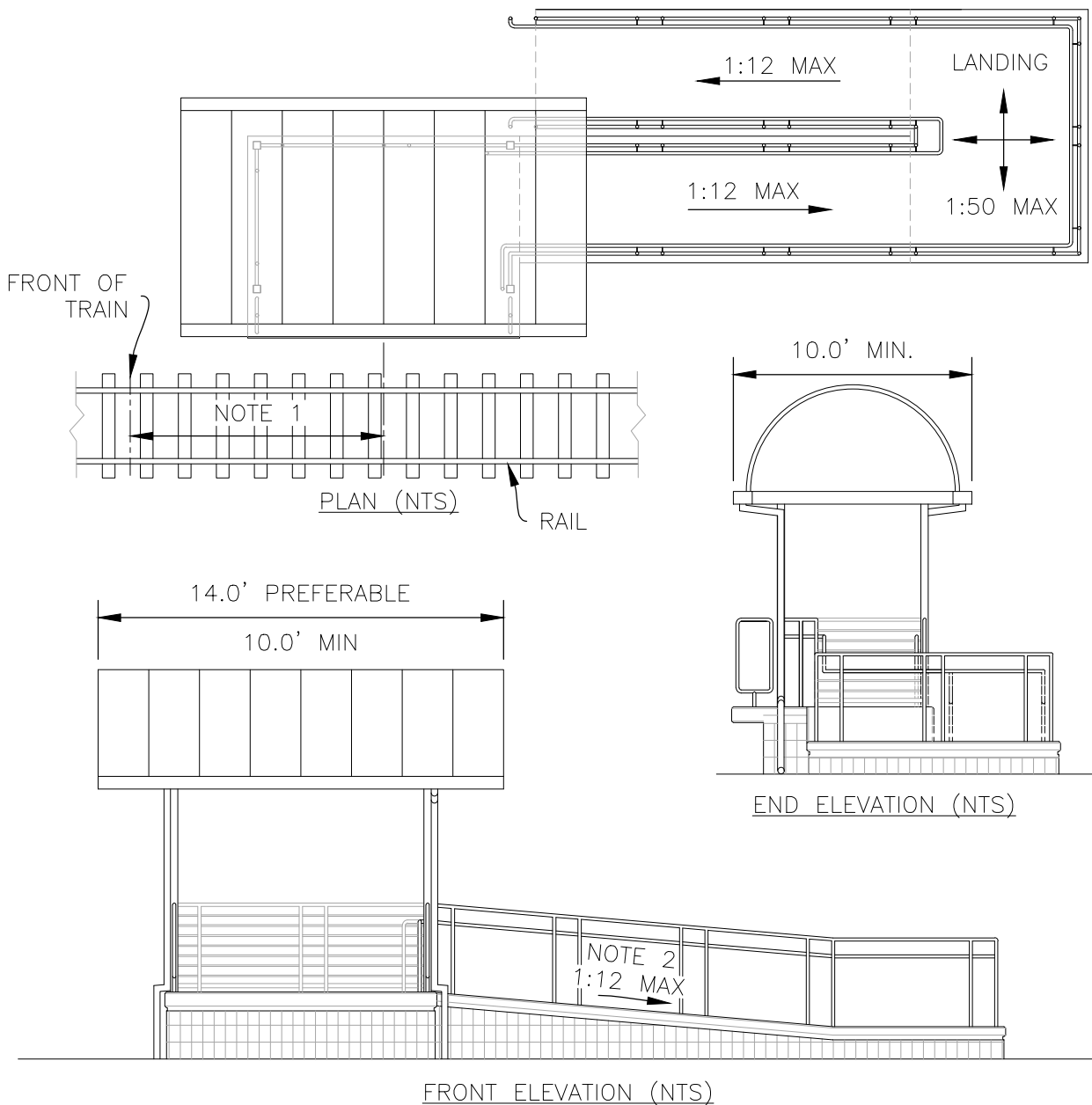
APPROVED:

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DATE:

FIGURE No.16-2(a)

LIGHT RAIL STATION SHELTER



NOTES:

1. MINI-HIGH PLATFORM SHALL BE 10.0' X 10.0' MINIMUM CENTERED 10.0' FROM THE FRONT TRAIN.
2. RAMPS SHALL BE 7% IN OPEN LOCATION OR 8.33% IN TIGHT LOCATIONS. WITH SITE CONSTRAINTS.



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**DESIGN GUIDELINES**

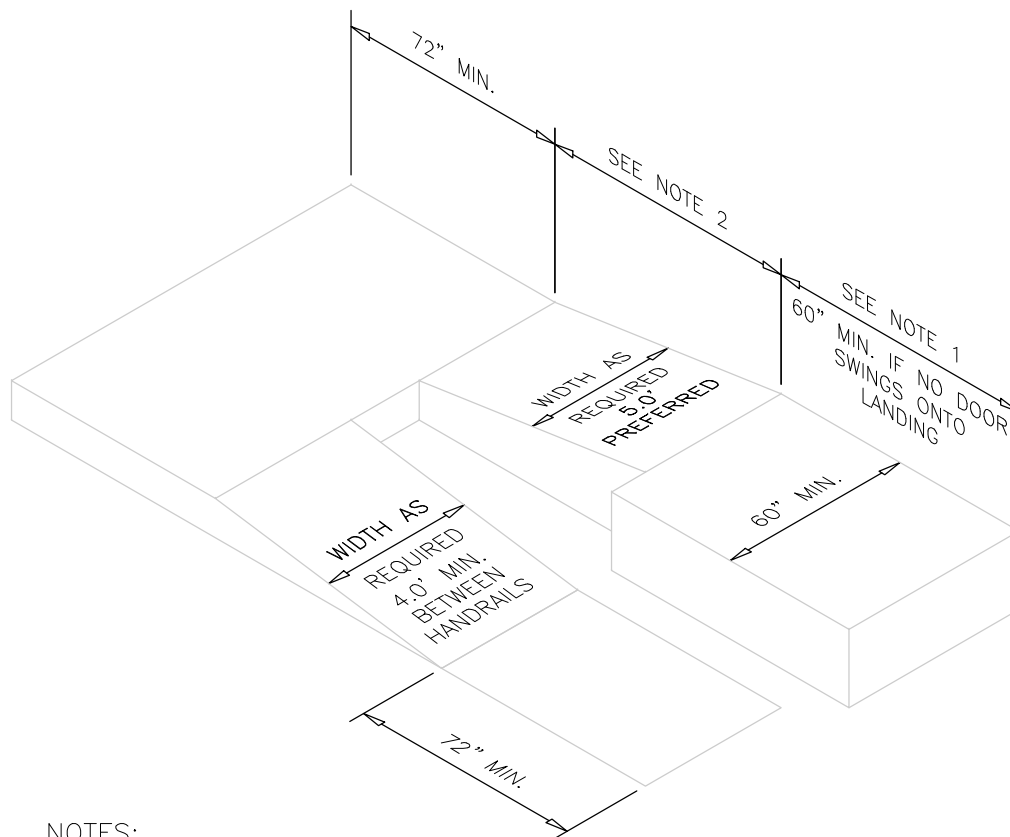
APPROVED:

*[Signature]*

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DATE: \_\_\_\_\_

**FIGURE No.16-2(b)  
LIGHT RAIL MINI-HIGH RAMP  
AND SHELTER**



NOTES:

1. WHEN DOOR SWINGS ONTO LANDING – 42" MIN. PLUS DOOR WIDTH
2. MAXIMUM HORIZONTAL DISTANCE OF EACH RAMP AND RUN VARY (MAX SLOPE 1:12)
3. LANDING AREAS SHALL BE SLOPED AT 2% OR LESS.



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**DESIGN GUIDELINES**

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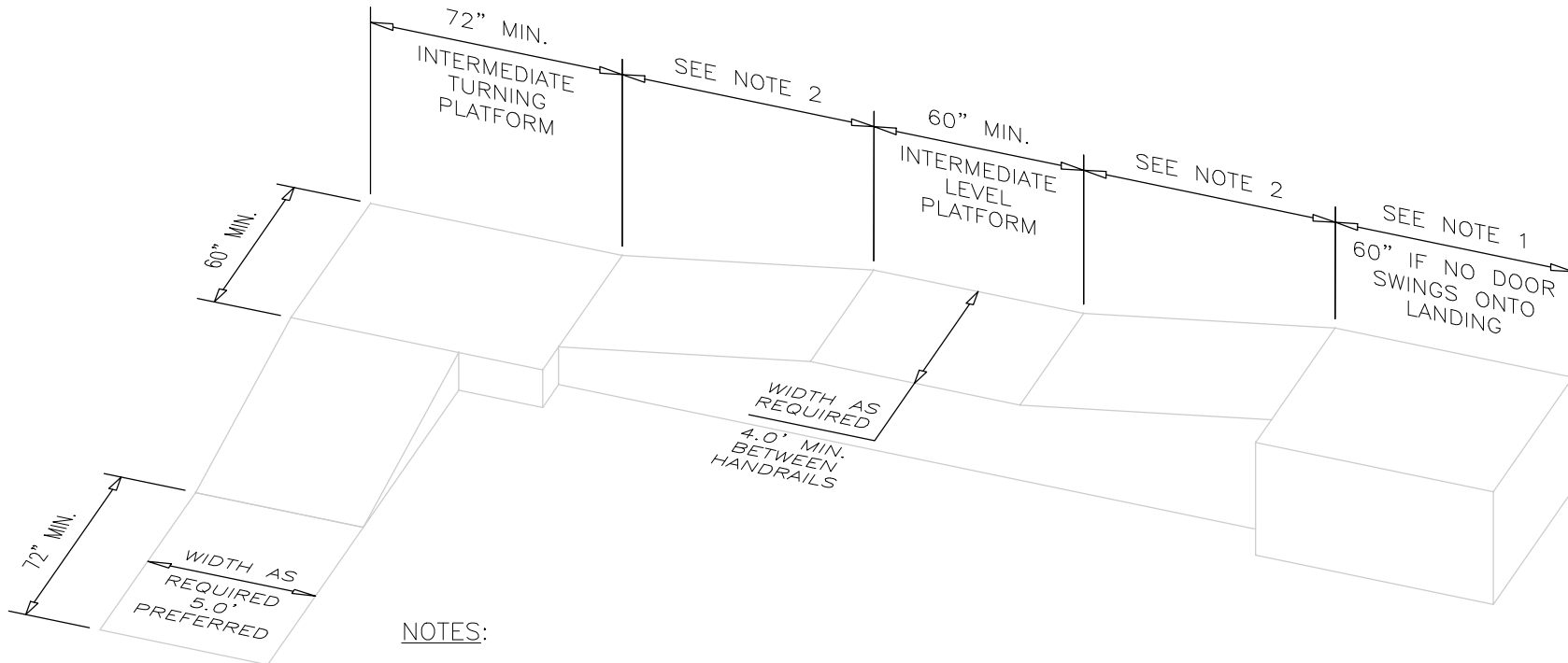
DIRECTOR, ENGINEERING AND CONSTRUCTION

DATE: \_\_\_\_\_

**FIGURE No.16-3(a)**

**ACCESS RAMP**

**WITH INTERMEDIATE SWITCH-BACK PLATFORM**



NOTES:

1. WHEN DOOR SWINGS ONTO LANDING – 42” MIN. PLUS DOOR WIDTH
2. MAXIMUM HORIZONTAL DISTANCE OF EACH RAMP AND RUN VARY (MAX SLOPE 1:12)

NOTE: WHEN THE DIRECTION OF RAMP CHANGES 30° OR MORE AT LANDING, THEN THE LANDING MUST BE A MINIMUM OF 72” IN LENGTH AND INCLUDE A MINIMUM 60” x 60” TURNING PLATFORM. LANDING AREAS SHALL BE SLOPED AT 2% OR LESS.



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**DESIGN GUIDELINES**

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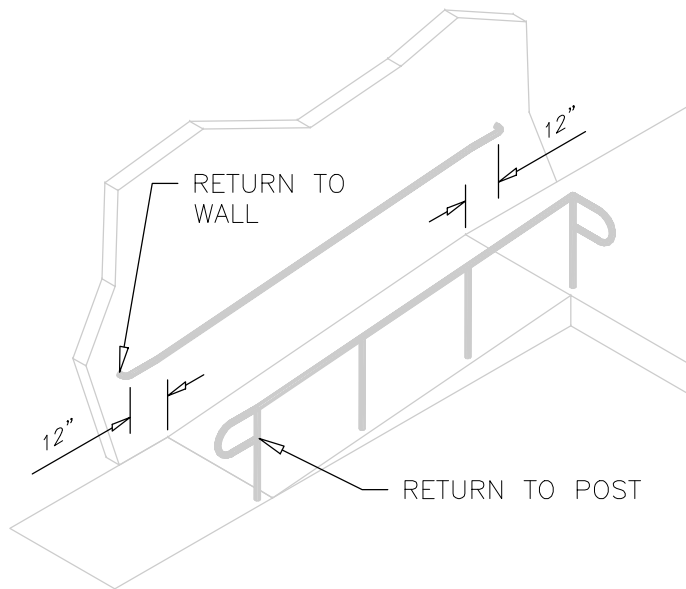
DIRECTOR, ENGINEERING AND CONSTRUCTION

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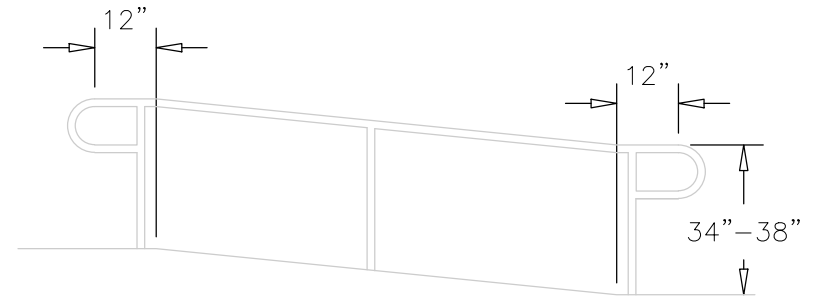
FIGURE No.16-3(b)

**ACCESS RAMP WITH TURNING PLATFORM  
(WHEN DIRECTION OF RAMP CHANGES 30° OR MORE)**

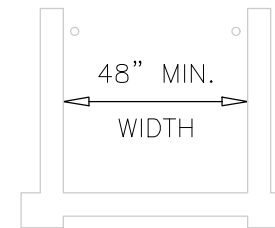
NOTE: HANDRAILS MUST EXTEND A MINIMUM OF 12" OF LEVEL DISTANCE (PARALLEL TO LANDING SURFACE) BEYOND TOP AND BOTTOM OF THE RAMP ENDS BEFORE THEY ARE RETURNED



WHERE THE EXTENSION OF THE HANDRAIL IN THE DIRECTION OF THE RAMP RUN WOULD CREATE A HAZARD, THE TERMINATION OF THE EXTENSION MAY BE MADE ROUNDED OR RETURNED SMOOTHLY TO A FLOOR, WALL OR POST



HANDRAILS MAY PROJECT INTO THE REQUIRED WIDTH A DISTANCE OF  $3\frac{1}{2}$ " FROM EACH SIDE OF A RAMP, OTHER PROJECTIONS, SUCH AS TRIM AND SIMILAR DECORATIVE FEATURES, MAY PROJECT INTO THE REQUIRED WIDTH  $1\frac{1}{2}$ " ON EACH SIDE



WIDTH MUST BE MINIMUM OF 60" WITH OCCUPANCY OF 300 OR MORE



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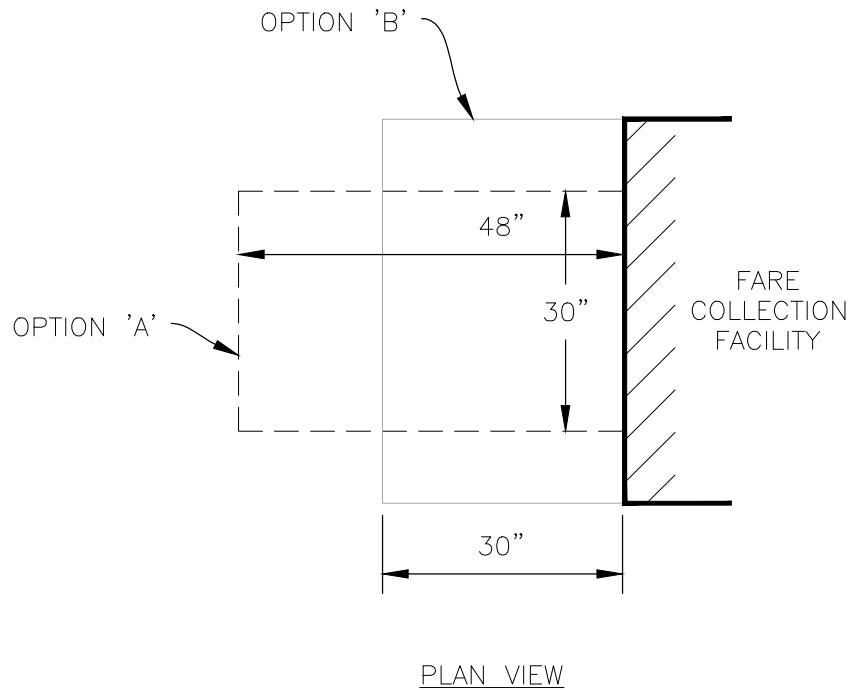
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FIGURE No.16-4

### HANDRAILS AT ACCESS RAMP



NOTE: THE DIMENSIONS INDICATE A SPACE FACING OPTION 'A' OR PARALLEL OPTION 'B' TO THE DEVICE.



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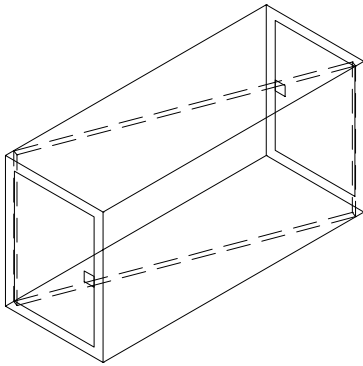
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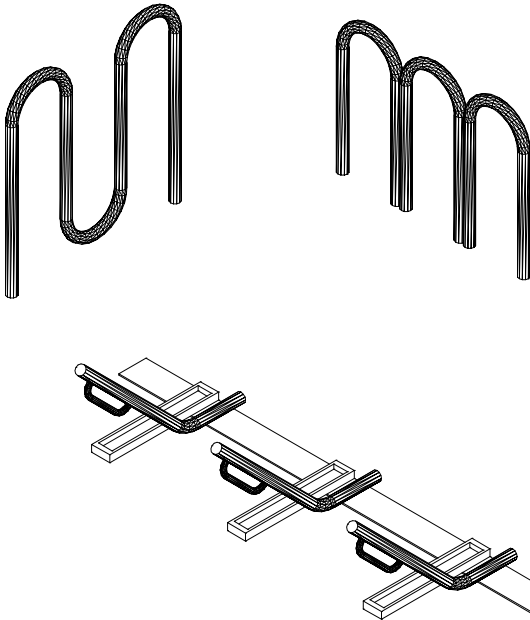
FIGURE No.16-5

**CLEAR FLOOR SPACE FOR  
FARE COLLECTION DEVICE**

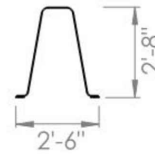
CLASS 1  
BICYCLE LOCKERS



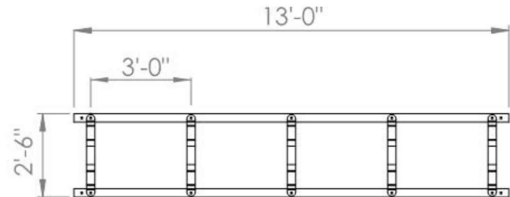
CLASS 3  
BICYCLE RACKS



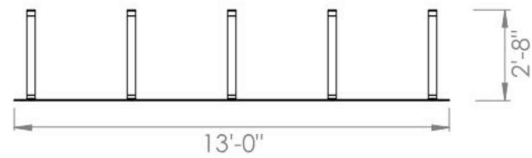
BICYCLE CORRALS



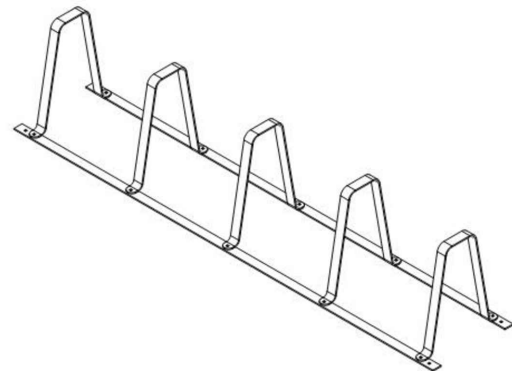
A. Side Elevation



B. Plan View



C. Front Elevation



D. Isometric Perspective

NOTES:

1. ALL BICYCLE PARKING FACILITIES SHALL BE SECURELY FASTENED TO THE GROUND WITH NON-REMOVABLE HARDWARE OR SHALL BE CAST INTO THE CONCRETE PAD.



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**DESIGN GUIDELINES**

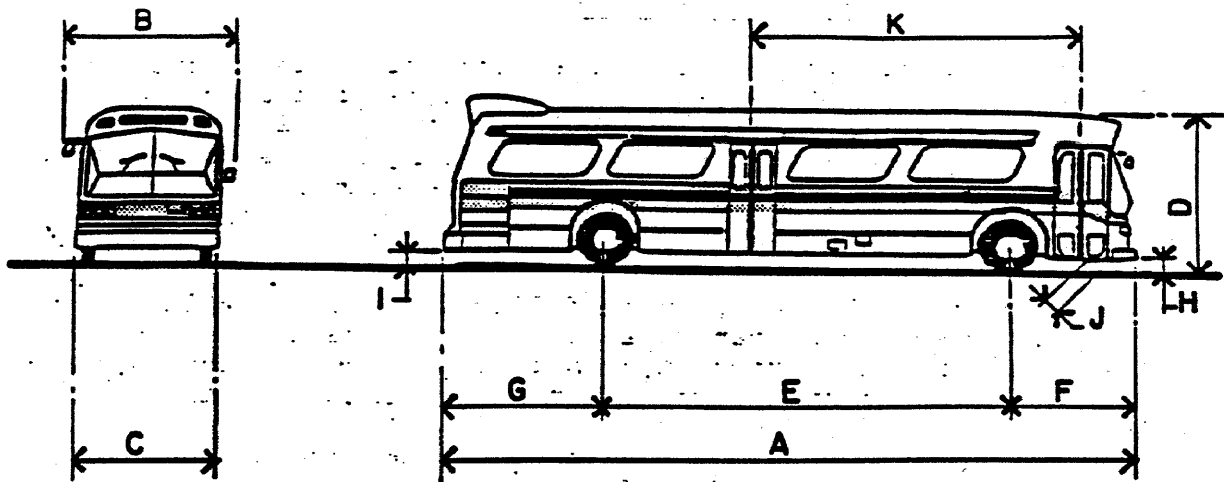
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**FIGURE No. 17-1  
BICYCLE PARKING FACILITIES**





**NOTES:**

	MINIMUM	MAXIMUM
A- OVERALL LENGTH	35'-0"	40'-0"
B- OVERALL WIDTH (WITH MIRRORS)	9'-6"	10'-0"
C- OVERALL WIDTH (WITHOUT MIRRORS)	8'-0"	8'-6"
D- OVERALL HEIGHT	10'-0"	10'-4 1/4"
E- WHEELBASE LENGTH	19'-0"	24'-0"
F- FRONT OVERHANG	7'-0"	8'-0"
G- REAR OVERHANG	7'-6"	9'-6"
H- FRONT BUMPER DISTANCE TO GROUND	1'-0"	1'-6"
I- REAR BUMPER DISTANCE TO GROUND	1'-6"	1'-10"
J- FIRST STEP DISTANCE TO GROUND	1'-0"	1'-0"
K- CENTERLINE FRONT DOOR TO REAR DOOR	16'-6"	22'-6"
L OUTSIDE TURNING RADIUS	32'-2"	37'-3"
GROSS VEHICLE WEIGHT ( POUNDS )	35,000	39,600
FRONT AXLE CAPACITY	12,300	14,300
REAR AXLE CAPACITY	22,700	25,300
PASSENGER CAPACITY		
• SEATING	41	51
• STANDING	20	26
TOTAL	61	77

SOURCE: REGIONAL TRANSIT FLEET ROSTER, REVISED JULY 25, 1986.



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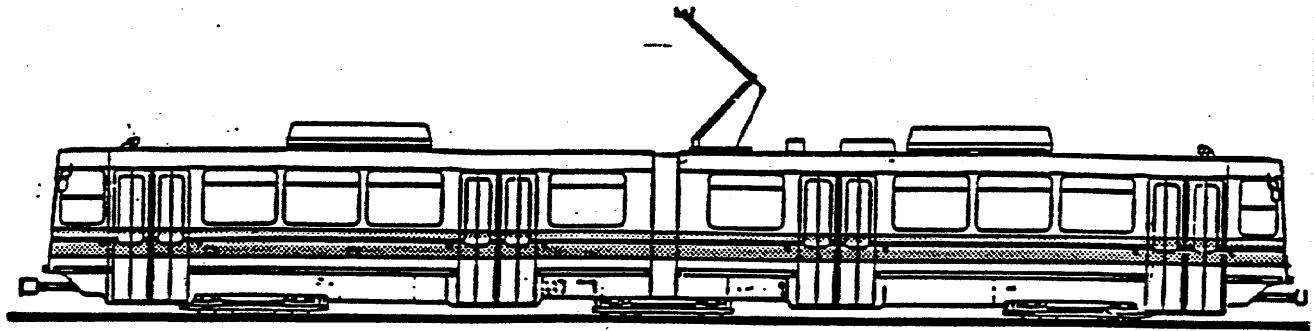
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**FIGURE No. A-1  
BUS SPECIFICATIONS**



**NOTES:**

HEIGHT (WITH PANTOGRAPH DOWN)	12'-6"
WIDTH (EXTERIOR)	8'-7"
LENGTH (INCLUDING COUPLERS)	79'-6"
WEIGHT (EMPTY)	77,800
SPEED (MAXIMUM PERMITTED)	50 MPH
CAPACITY	
• SEATED	64
• STANDING	111
• TOTAL	175



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FIGURE No. A-2

**RT METRO  
VEHICLE SPECIFICATIONS**

**2017 APTA INTERNATIONAL BUS ROADEO  
MAINTENANCE INSPECTION AND PRE-TRIP BUS  
40' - COMPETITION BUS**



<b>2015 Gillig Low Floor BRT Plus CNG</b>	
<b>Unloaded Weight</b>	31,900 pounds
<b>GVRW</b>	41,600 pounds
<b>GVRW Front</b>	14,600 pounds
<b>GVRW Rear</b>	27,000 pounds
<b>Overall Length</b>	40'
<b>Turning Radius</b>	43' 3" at the bumper
<b>Overall Height</b>	135 inches
<b>Overall Width</b>	102 inches
<b>Front Step Height</b>	16 inches
<b>Approach/Departure Angle</b>	N/A
<b>Engine</b>	Cummins ISL G 8.9
<b>Transmission</b>	Voith D864.5
<b>Advertised Horsepower</b>	280HP
<b>Peak Torque</b>	900 LB-FT
<b>Governed Speed</b>	2200 RPM
<b>Number of Cylinders</b>	6 Cylinders
<b>Mirrors</b>	Left: 8" x 11", Right: 8" x 15"



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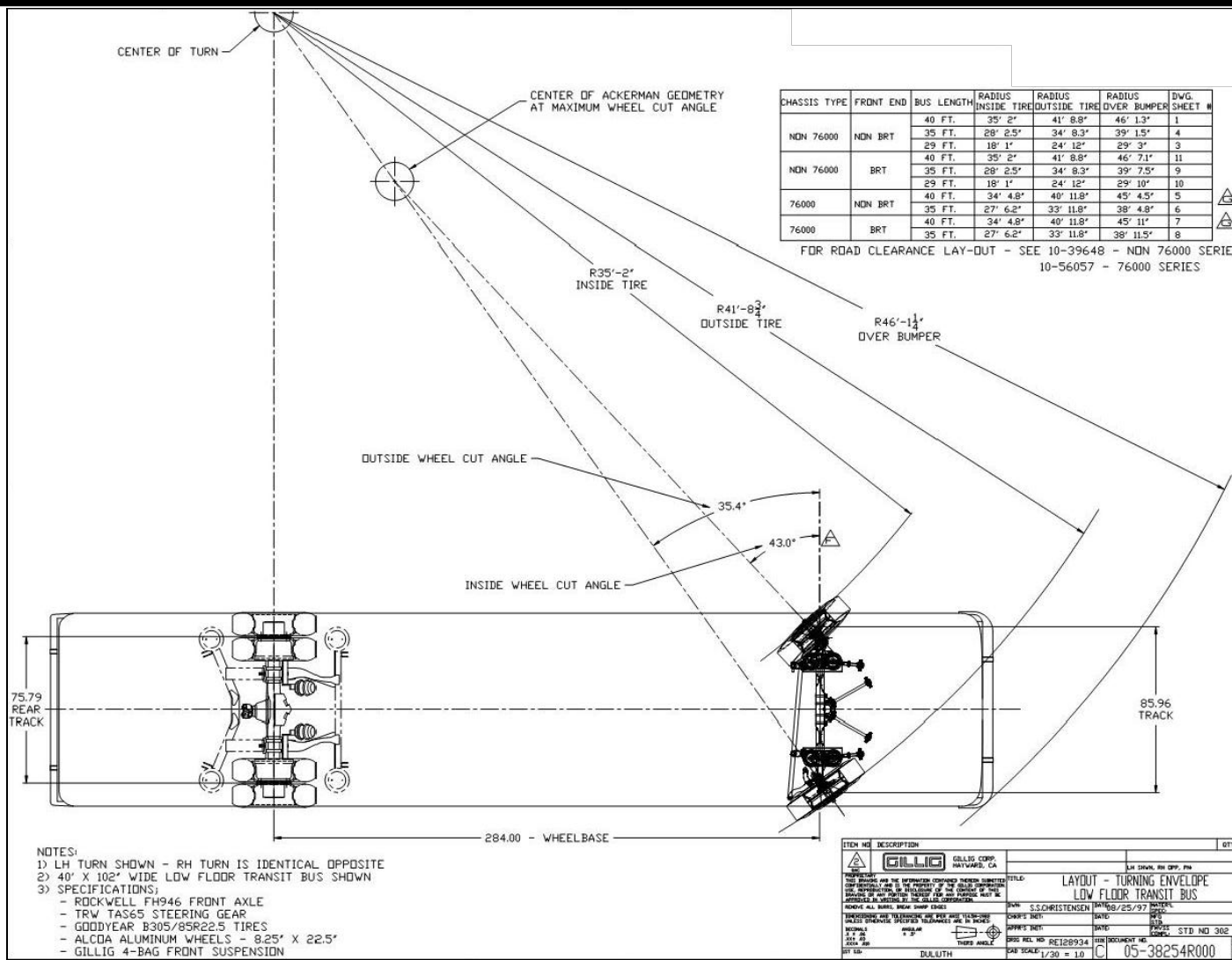
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**FIGURE No. A-3(a)  
GILLIG BUS SPECIFICATIONS**



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*[Signature]*

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FIGURE No. A-3(b)

## GILLIG BUS TURNING SPECIFICATIONS